

Hood Street Precinct Design Guidelines



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1.0 Introduction





1.0 Introduction

1.1 Introduction & Context

The Hood Street Precinct Design Guidelines have been adopted by the Subiaco Redevelopment Authority (the Authority) to set out the building and development design requirements to facilitate appropriate redevelopment of the Hood Street Precinct of the Subiaco Redevelopment Area. The Hood Street Precinct Design Guidelines work together with the Subiaco Redevelopment Scheme and the Planning Policies adopted under the Redevelopment Scheme to provide the statutory planning framework for the Hood Street Precinct.

The Authority was formed in 1994 by an Act of Parliament to revitalise 80 hectares of under utilised industrial land surrounding the Subiaco Train Station. The Authority's vision is to create a vibrant and sustainable community, based on best practice in urban renewal and design, including exemplary transit oriented development. The Authority is responsible for all aspects of the revitalisation, from master planning and precinct planning, to subdivision, land sales, development control and place management.

To date, 86% of the Subiaco Redevelopment Area is complete and returned to the City of Subiaco for ongoing management. The Authority is continuing to plan for and progress the redevelopment of the remaining area, including the Centro North, Australian Fine China and Hood Street Precincts.





1.2 The Hood Street Precinct

The Redevelopment Area, known as Subi Centro, consists of 10 precincts clustered around the Subiaco Train Station and the former industrial area to the north-west of the station. The boundaries of each precinct are set out in the Subiaco Redevelopment Scheme, and Design Guidelines are prepared to guide development of each precinct.

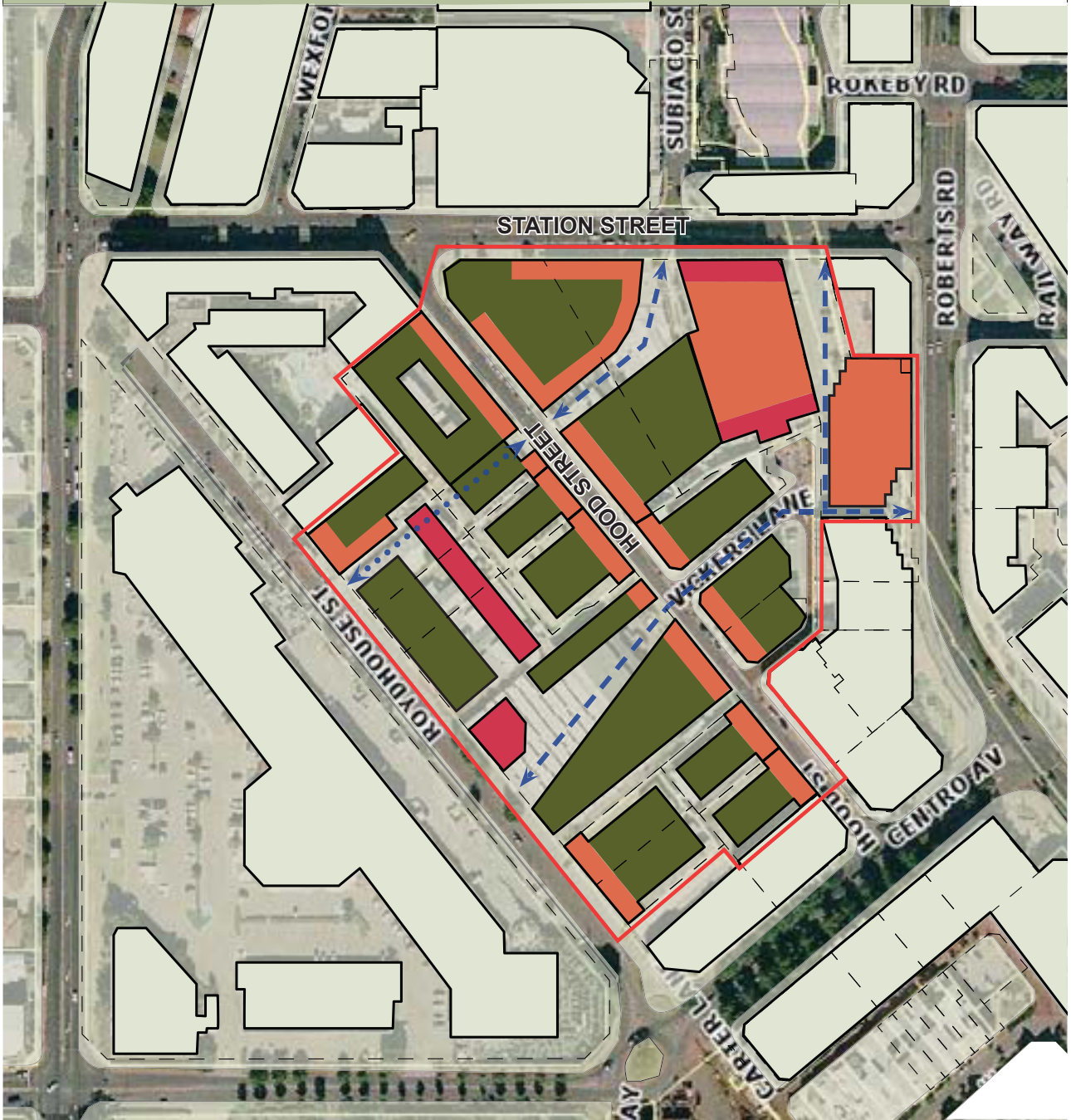
The Hood Street Precinct was created in 2011 through the amalgamation of part of the Roydhouse Precinct and Centro Place Precincts. This new precinct was created to facilitate the redevelopment of the last remaining industrial and vacant lots within the two precincts. The Hood Street Precinct covers approximately 4ha and includes the majority of lots within Hood Street and along the southern side of Roydhouse Street, as shown on the Precinct Plan.

The Hood Street Precinct Plan was prepared by the Authority during 2010/11 through a collaborative design practice where key stakeholders, including landowners, relevant government agencies and business groups, were encouraged to be involved in the design process. The Precinct Plan reflects the combined stakeholder aspirations for the area, which included more residents, higher and denser built form and more activity, as well as better connections and appropriate land use allocation.

The vision for the Hood Street Precinct is to compliment the urban renewal of Subi Centro through the redevelopment of the precinct including quality mixed use development and improved connections to the surrounding area. The precinct will build on Subi Centro's award winning Transit Orientated Development, through increased commercial and residential development and strong connectivity to the Subiaco Train Station. Improved vehicle and pedestrian access through the precinct will link residents and workers within the precinct and north-west of the precinct to the train station.

HOOD STREET PRECINCT

Building Height



1.3 Application of Design Guidelines

These Design Guidelines are a statutory planning document adopted under the powers of the Subiaco Redevelopment Scheme. They have been prepared to guide development within the Hood Street Precinct to ensure a high quality of development within the Redevelopment Area.

This document forms one part of a series of documents relevant to the planning and development of the Hood Street Precinct. The Design Guidelines are to be read in conjunction with the Subiaco Redevelopment Scheme and Subiaco Redevelopment Authority Planning Policies. In determining any application for development approval, the Authority will utilise these Design Guidelines, in conjunction with the Scheme and Planning Policies, as the primary assessment criteria. Any variations proposed to the requirements within the Design Guidelines will require full and substantiated justification by the applicant.

Other legislation and regulations apply to development within the precinct and should be read in conjunction with these Design Guidelines. This includes but is not limited to the Building Code of Australia, Disability Discrimination Act, Health Act, Environmental Protection Noise Regulations and City of Subiaco Local Laws.

1.4 Application of Planning Policy

In addition to the Subiaco Redevelopment Scheme and these Design Guidelines, the Planning Policies adopted under the Redevelopment Scheme also apply to development within the Hood Street Precinct. Applicants are advised to familiarise themselves with the requirements of the Planning Policies prior to commencing building design.

In accordance with Planning Policy 1.18 Affordable and Diverse Housing, Design Guidelines are required to set out which sites the affordable housing requirements apply to. **The affordable housing requirements of Planning Policy 1.18 Affordable and Diverse Housing applies to all sites in the Hood Street Precinct.** The policy requires that all development applications that include ten (10) or more residential dwellings are to provide a minimum of 12% of dwellings as affordable housing, for disposal as either social housing or affordable owner occupier housing.

As increased residential development is a key objective of the Hood Street Precinct, the Authority may consider approving a variation to applicable plot ratio limits to offset the potential loss of profit associated with the requirement to provide affordable housing, where it is considered by the Authority that the variation will not result in inappropriate built form. The maximum variation will be at a rate of 1m²:1m²; i.e. for every square metre of affordable dwelling floor space provided, a plot ratio bonus of one square metre may be included within the site development. The plot ratio bonus only applies where at least 12% affordable housing is provided on the site and is not available where cash-in-lieu is provided. Approval, or refusal of any plot ratio bonus does not affect the requirement to always provide a minimum of 12% affordable housing in all applicable developments.

Please refer to the Planning Policies for further details of all policy requirements.



1.5 Design Development and Approval Process

The Authority aims to ensure that all land owners, their consultants and design teams are aware of the design objectives for the precinct. Please refer to the following steps, which outline the design development and approval process required for development in the Hood Street Precinct

- Step 1** The developers and their project team meet with the Authority to discuss design requirements, the Authority will provide written feedback on indicative plans if requested.
- Step 2** The applicant lodges the Development Application with the Authority, addressing the requirements of the Scheme, Design Guidelines and Planning Policies.
- Step 3** The Development Application is referred to City of Subiaco, Town of Cambridge and other government agencies as required, for a 42 day comment period.
- Step 4** The Authority's independent Design Review Panel assesses the design.
- Step 5** The Authority works with the applicant to address issues that may require design amendments to meet statutory planning requirements.
- Step 6** The Authority determines the Development Application.
- Step 7** The developer lodges Working Drawings with the Authority, demonstrating compliance with the plans and conditions of the Development Approval.
- Step 8** The developer lodges a Building Licence application directly with City of Subiaco.
- Step 9** The Authority issues Working Drawings clearance when all development approval conditions are met.
- Step 10** The City of Subiaco issues a Building Licence.
- Step 11** The developer undertakes construction.
- Step 12** The Authority undertakes an audit of the completed development to ensure compliance with the development approval and working drawings.



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The image features a large, solid orange rectangle in the center, which contains the text "2.0 Public Realm" in a bold, white, sans-serif font. This rectangle is set against a background of a light gray grid pattern that is partially obscured by a large, light green, semi-transparent shape in the bottom right corner. Above the orange rectangle, there is a horizontal band of six colored squares: green, orange, light green, brown, dark blue, and red. Below this band, there is another horizontal band of six colored squares: red, brown, dark blue, light green, orange, and green. The overall design is modern and abstract, with a focus on geometric shapes and a color palette of earthy and vibrant tones.

2.0 Public Realm

2.0 Public Realm

Section 2 sets out the design guidelines for the development of public land and the development of land that provides public access. All development on private land should also be mindful of the importance of the public realm in providing a quality urban experience - this is discussed in more detail in Section 3.

2.1 Principles for Development of the Public Realm

Any development on land within the public realm (publicly accessible land such as public or private roads, pedestrian access ways, public plazas or malls, parks or other public spaces) are to be designed and developed in accordance with the following principles. These principles apply regardless of land ownership or tenure and includes land owned or managed by State or local government, utility service agencies, a private developer, or private institution.

- Activation of the public realm is an essential component of the vision for the Hood Street Precinct Development within and adjacent to the public realm is to be designed to encourage and facilitate activation through public use and enjoyment of the public realm.
- Designing safe, attractive and interesting pedestrian paths of travel. Pedestrian accessibility is to provide efficient connectivity through the Precinct and to key destinations, particularly the Subiaco Train Station.
- Creating inviting, attractive and safe public places where people are encouraged to meet, relax or socialise.
- Incorporating active land uses into or adjacent to the public realm where possible, such as cafes, alfresco dining, markets or shops with active frontages.
- Design and development within the public realm is to incorporate design principles Crime Prevention Through Environmental Design (CPTED).
- Development of the public realm is to be designed to provide universal access and meet the requirements of the Disability Discrimination Act 1992 and relevant provisions of the Building Code of Australia and Australian Standards.
- Water Sensitive Urban Design (WSUD) principles are to be applied to the management of water within any public open space or road reserve.



2.2 Intended Street Connections and Pedestrian Access Ways

The Hood Street Precinct Plan identifies four intended street connections that can be created through redevelopment of the Precinct, either as new public roads or pedestrian access ways. These connections are an important part of the vision for the precinct, as they will improve connectivity and permeability through the Precinct. This will also provide improved access to the train station and to Subiaco Square.

Creation of these intended street connections also provides benefits to the development sites. Designing developments to incorporate these connections will reduce building bulk, break up built form and provide better design opportunities. The connections will provide increased ground floor frontages for commercial uses, increased accessibility for residential development, and improved solar access and ventilation for buildings.

Development of these connections is to meet the following requirements:

- Any public access way or public road is to be designed and developed in accordance with section 2.1 Principles for Development of the Public Realm.
- The required width of each intended connection is set out in Section 5.0 Site Specific Requirements. The detailed design of each connection is to be addressed through discussions with the Authority and the City of Subiaco at development application and working drawings stage.
- To deliver the intended connections in the redevelopment of private land, the Authority will consider approving a variation to the maximum plot ratio provisions of the Scheme and Design Guidelines to compensate for the loss of developable area within the lot. Variations are not provided as of right and the development application must demonstrate an appropriate design response for the site, with any built form variations appropriate for the context.
- Any access way that will provide vehicle access is to become a public road. The land is to be ceded to the City of Subiaco in accordance with their processes and requirements for public roads.

Further detail of the intended connections is also shown in Section 5 Site Specific Requirements.





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3.0 Building Design

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All development within the Hood Street Precinct is to be designed to meet the following building design criteria 3.1 to 3.10

3.1 Architectural Design

- All new developments are to be of a high quality, contemporary architectural design, that responds to the context of the development and the established character and quality of Subi Centro.
- Architectural design and building detail is to be used to provide strong articulation of buildings and reduction in building bulk. This is to include variation in building plane and the use of architectural features to punctuate buildings. Variety in materials, colours, textures and other detailing should also be used to create fine grain detail and create visual interest.
- All buildings must address adjacent streets, utilising major windows, shop fronts, balconies, pedestrian entrances, awnings and other similar elements to enrich and activate the streetscape.
- All buildings are to be designed in a manner that maximises solar access and passive ventilation and minimises overshadowing of adjacent buildings.



Image: Scene Apartments - Gresley Abas Architects



3.2 Materials and Finishes

- All developments are to include a variety of high quality, durable materials and finishes, that produces a quality building finish with detail and visual interest.
- Blank external walls are not permitted; all external walls are to include articulation, visual detailing and quality finishes.
- Buildings are to incorporate a mix of external materials; where a large area of single material is proposed, it is to be broken down into smaller elements.
- A schedule of proposed colours and materials is to be provided to the Authority as part of any development application.



3.3 Activation of the Streetscape

- All buildings are to be designed to engage with and activate the public realm, particularly at street level.
- Building design and architectural features are to be used to create building articulation and visual interest, such as windows, balconies, awnings, building entrances and modulation in building plane.
- Built form patterns and horizontal and vertical elements should be used to create a consistent streetscape rhythm, including breaking down large built form elements into smaller components and providing consistent floor heights between adjacent buildings.
- Buildings should include active ground floor uses, such as commercial and retail use, with residential development generally limited to upper floors.
- Ground floor land uses should have predominately glazed frontages, with activity located behind it. Awnings and other shading devised should be used where required rather than dark or reflective window tinting.
- Pedestrian paths of travel, shop fronts, awnings and pedestrian entries to all buildings are to be well lit.

3.4 Design for Safety

- Developments are to incorporate design principles of Crime Prevention Through Environmental Design (CPTED).
- Security and surveillance design measures are to include active street frontages at street level and passive surveillance from upper floor windows and balconies.
- Safety design features are to include external lighting, safe entrances, eliminating entrapment points, and safe commercial loading and storage areas.

3.5 Roof Form

- Roof forms should be of a contemporary nature and designed to complement the building. A mix of pitched, flat or skillion roofs can be incorporated into the Hood Street Precinct.
- Roof plant and lift overruns should be carefully considered in roof design and either incorporated as an integral part of the roof design or concealed from view.

3.6 Corner Lots

- Developments are to acknowledge the intersection of the adjoining streets through their position and massing on the site. Architectural expression on the corners of buildings is to be created through elements such as the orientation of the building, height differentiation, architectural features and materials.



3.7 Access

- All building entrances are to be clearly defined, safe and well lit. Buildings should use architectural features to establish visually distinct pedestrian access points.
- Building entries for retail and commercial development are to be at-grade to the adjacent street or footpath. Residential entries should be at or near street level.
- A single point of vehicle access is to be provided for each development. A visual truncation of 2m x 2m is required for vehicle exits points.

3.8 Awnings

- For all new mixed use and non residential buildings an awning is to be provided along the entire length of all street facades that front footpaths or walkways.
- The height and street setback of awnings are to be in accordance with the requirements of the City of Subiaco.
- The height and width of awnings on new buildings should be consistent with existing awnings on adjacent buildings, where possible.

3.9 Residential Private Open Space

- Private open space must be provided for each residential dwelling; it is to be directly accessible from a living space, be oriented for access to northern sunlight where possible and be of a useable size and dimension:
 - The minimum sizes for balconies is 10m² with a minimum dimension of 2.5m.
 - The minimum size for ground floor courtyards is 15m² and 3.0m dimension.
- Balconies are to be sensitively located or screened to avoid any potential visual privacy impact between dwellings within the lot or on adjoining lots. If screening is necessary, it should be integrated into the building design and must not unduly add to the building bulk.
- Balconies must be contained within the lot boundary and may not extend into the existing or proposed road reserve.



3.10 Sustainable Design

- All new buildings are to be designed to demonstrate ability to achieve a minimum four star Green Star rating ('as designed' rating) from the Green Building Council of Australia (GBCA), in accordance with the following:
 - Where a Green Star Rating tool has been adopted by the GBCA or where the applicable Green Star rating tool is in pilot phase with the GBCA, the applicant is to engage a Green Star Accredited Professional to undertake self-assessment of the development against the tool and a compliance statement is to be submitted to the satisfaction of the Authority at Working Drawings stage.
 - To ensure that the Green Star design rating is achieved in construction of the development, a statement from a Green Star Accredited Professional will be required to be submitted to the Authority at practical completion stage of all developments and prior to occupation of the building. The statement is to confirm that all initiatives identified in the design certification have been implemented.
- The Authority may consider a proposed alternative rating system to Green Star (such as NABERS) where the applicant demonstrates that the rating system and the design of the development is generally equivalent to or better than the four star Green Star rating.
 - All new buildings (except single residential) are encouraged to include "green roofs" (i.e. roof vegetation) to enhance thermal benefits, reduce stormwater generation and enhance the soft landscape aesthetic of the development. Where possible the green roof should be accessible to building occupants for use as a roof top garden.
 - All habitable rooms (i.e. bedrooms and lounge rooms) are to be provided with direct access to natural light (not borrowed light) and where possible provided with cross ventilation.
 - A landscape plan is to be submitted with the development application that has been prepared in accordance with the Water Corporation's Waterwise Development criteria for landscaping, such as use of native and water-wise plants and irrigation and rain water management.
 - Development applications for refurbishments or additions to existing buildings are to be accompanied by a statement identifying the manner in which the proposal addresses sustainability, such as disposal / reuse of demolition material and construction waste and the use of sustainable materials.





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4.0 Servicing and Ancillary



4.0 Servicing and Ancillary

The following servicing and ancillary design guidelines are to be applied to all relevant aspects of new buildings and development of existing buildings within the Hood Street Precinct.

4.1 Vehicle Parking

- Car, motorcycle and bicycle parking for all developments is to be provided in accordance with the Parking Requirements Table set out below. A Traffic Impact Statement is required to be submitted with any Development Application seeking a variation to the requirements.
- Parking is not to be visible from the street and is to be provided at basement level or concealed behind the built form.
- Short term parking for delivery and service vehicles is to be provided for any land use that generates frequent deliveries or collections, such as large retail stores, supermarkets and restaurants.
- On-site visitor car parking is not required for developments within the Precinct.
- A parking and traffic management plan is to be submitted for any development with 10 or more parking bays and/or a developments with loading / service areas.

Parking Requirements Table			
Land Use	Multi-Unit Residential	Commercial/Retail/Other	Single Lot Residential
Max. Car parking	1.4 bays per dwelling (average)	As per Scheme	2
Min. Carparking	1 bay per dwelling	As per Scheme	1
Min. Motorbike/Scooter Parking	1 bay per 10 car bays provided	1 bay per 10 car bays provided	n/a
Min. Bicycle Storage	Provided within 6m ² residential storage space.	1 secure bicycle storage space per 200m ² NLA	n/a

4.2 End of Trip Facilities

Any new building that includes retail, commercial or any other non-residential development is to include end of trip facilities to support other modes of travel such as running, walking and cycling. There is to be one locker per bicycle storage space provided and one shower for every 10 bicycle storage spaces with a minimum of one shower to be provided where less than 10 bicycle spaces is provided.

4.3 Storage for Dwellings

A minimum lockable storage area of 6m² with a minimum internal dimension of 1.5m is required for each dwelling in all multiple residential and mixed-use developments. A minimum of 6m² is required as the storage area includes provision for bicycle storage. The size of the store may be decreased to 4m² if bicycle storage is provided elsewhere.

4.4 Waste Collection

- All buildings are to include waste collection areas to suit the City of Subiaco bin size requirements and waste reduction and recycling initiatives.
- Waste collection areas / bin enclosures should be located within basements where possible, or behind the primary building line and screened from public view with a quality material compatible with the building design.

4.5 Storm Water

All stormwater shall be contained on-site within each development. Storm water retention is to be shown on development application and/or working drawings plans.

4.6 Fencing

- For most developments buildings will be built up to the lot boundary and will not require fencing. Where fencing is proposed on any street or public realm boundary it is to be a maximum height of 1.2 metres at the boundary and a minimum of 70% visually permeable.
- Fences are to be constructed in a quality material, compatible with the building design.

4.7 Building Services

- Balconies to residential apartments are provided for the resident's amenity and should not be used for building services. Where air conditioners or clothes drying areas are proposed on balconies an additional area (in addition to the minimum 10m² under section 3.10) is to be provided and screened from public view.
- Air conditioning units must not be visible from the street. They should be located in the basement of buildings, or if this is not possible, located on roofs and screened from view. Locations must be mindful of noise generated and adjacent residential or sensitive land uses and must comply with the Environmental Protection (Noise) Regulations 1997 and the Authority's Sound Attenuation Policy.
- Fire booster cabinets, services boxes, solar panels and other service infrastructure are to be designed to integrate into the building and minimise visual impact on the streetscape. Any service infrastructure in front setback areas is to be screened from view with a quality screening material.
- TV antennae are to be located within the roof space wherever reception permits. Where this is not possible, antennae and satellite dishes are to be located behind the roof ridgeline with minimised visibility from the street and neighbouring properties. Multiple dwellings are to be provided with one antennae or dish servicing several dwellings.

4.8 Encroachments

All building components that comprise habitable space must be located within the lot. Building components that comprise non-habitable space, such as awnings, architectural features or sustainability features that encroach beyond lot boundaries may be considered, subject to approval of relevant authorities such as City of Subiaco and the Department of Regional Development and Lands.

4.9 Zone of Influence Specifications

Lot 13754 Roberts Road is within the zone of influence above the Subiaco railway tunnel, and as such, certificates of title should be checked for the exact location of the zone of influence prior to any development at the site.

The following load limits apply in the zone of influence over the railway tunnel:

- (a) 20 kPa (allowance for 1 metre of compacted backfill or pavement) plus;
- (b) building load:

Alternative Load Combinations	Load Type
(i) Blanket uniformly distributed load; or	20 kPa D.L. + 15 kPa L.L.
(ii) Point load on 7.5m x 7.5m grid plus uniformly distributed load on adjacent area; or	6 kPa D.L. + 4 kPa L.L. + 1000 kN/m D.L. + 400 kN/m L.L.
(iii) Line load at 7.5m centres plus uniformly distributed load on adjacent area	6 kPa D.L. + 4 kPa L.L. + 130 kN/m D.L. + 70 kN/m L.L.

Notes: 'Line' Loads to be distributed over a contact width of not less than 300mm.
'Point' Loads to be distributed over a contact area of not less than 1.0m x 1.0m.



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5.0 Site Specific Guidelines

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Section 5 sets out the specific built form, design and land use requirements for each lot within the Precinct. Sections 5.1-5.5 provide definitions/explanations of standards that are to be applied to the specific requirements for each lot set out in the 5.6 Table 1 Site Specific Requirements.

5.1 Building Height

Building height is defined by the maximum number of storeys up to a maximum height in metres, as prescribed in Table 1. Height shall be measured from the existing ground level at the boundaries of the site to the top of the roof of the building, excluding minor protruding architectural features. Roof plant and lift over-runs and roof terraces may be contained above the maximum building height but must be set back from boundaries so as not be in the line of sight from ground level.

5.2 Setbacks

The general intent for the Precinct is to for buildings to have minimal front setbacks, bringing the built form to property boundaries and creating an interface between the public and private realm. In some cases it will be necessary to vary front setbacks in order to create interesting built forms, particularly for larger buildings.

Table 1 includes side and rear setback requirements for most lots, as well as requirements to setback any building elements above four storeys.

5.3 Plot Ratio

Plot ratio defines the permissible floor area for each development. Unless stated otherwise plot ratio area is to be calculated from the ground level lot area. Aspects such as balconies, lobbies and parking at basement and ground floor level are not included in the calculation of plot ratio area - refer to the Redevelopment Scheme for a detailed definition of plot ratio.

5.4 Land Use and Residential Density

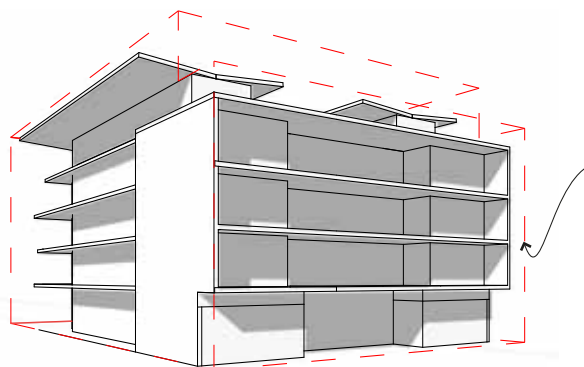
The Redevelopment Scheme Preferred and Contemplated land uses apply to the Precinct, as noted in Table 1, the Preferred land uses are Commercial and/or Residential. However, refer to Table 1 to determine appropriate mixture of uses and specifically the required ground floor uses.

A Residential Density Code does not apply to the precinct.

5.5 Building Envelopes

Building envelope diagrams are provided for each group of lots as shown in the site plan. The lots have been grouped according to proximity to one another and shared characteristics.

The building envelope volumes demonstrate the maximum extents of built form but do not represent actual buildings to be developed. In detailed building design these envelopes will be moderated by a design that balances all considerations – including lot boundaries, maximum plot ratio, open space requirements, access to daylight and ventilation and architectural treatments.



Example of a building designed to maximise plot ratio, access to light and ventilation and to fit within the allowable building envelope.



5.6 Table 1 - Site Specific Requirements

Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 35 Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 storeys Station Street: Nil - Min 4m above 4 storeys Intended pedestrian connection: Nil - Min 4m above 4 Storeys	3.0:1	Commercial Residential	Hood Street Possible service vehicle access after hours via intended pedestrian access way
	Development Intent: Mixed use commercial and residential building with active uses to Hood Street, Station Street and to the intended pedestrian connection shown on lot plan. Corner location has potential to develop hospitality uses and small floor plate retail.			Other Design Requirements: Corner element of up to 5 storeys permitted at intersection of Hood and Station Streets - maximum floor area 200m ² . The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.	
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 36 Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Nil permitted	3.0:1	Commercial Residential	Hood Street
	Development Intent: Mixed use, commercial and residential building with active uses to Hood Street and to the intended pedestrian connection shown on lot plan			Other Design Requirements: Part of the intended pedestrian connection shown on the lot plan connecting Hood Street and Station Street is located on the north eastern portion of lot 36. The connection is to be an active pedestrian oriented access way which improves permeability in the precinct and access to Subiaco Station. The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.	
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 40 Station Street	4 storeys up to 15m 5 storeys up to 18m	Setbacks vary; refer to lot plan and diagrams	3.0:1	Commercial Residential	Vickers Lane
	Development Intent: Mixed use commercial and residential building with active uses to Station Street and to the intended pedestrian connection shown on lot plan.			Other Design Requirements: Lot 40 is the current location of the Station Street Markets. Maintaining this use in future developments is encouraged and special consideration / concessions may be given to applications that incorporate a market on the site. The Southern portion of Lot 40 contains a recently developed commercial building with north facing windows. If this building is to be retained future development on the northern portion of the lot must not present large expanses of blank walls to, or overshadow the existing development. The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.	

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Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 1 (No 27) Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 storeys Vickers Lane: Nil permitted Side: Nil permitted Rear: Nil permitted	3.0:1	Commercial Residential	Only via existing rear entry off Vickers Lane
Development Intent: Mixed use commercial and residential building with active uses to Hood Street and to the intended pedestrian connection shown on lot plan					
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 13754 Roberts Road	4 storeys up to 15m	Setbacks vary; refer to lot plan and diagrams	3.0:1	Car parking Commercial Residential	No vehicular access from Roberts Road.
Development Intent: The lot is currently owned by the Subiaco Redevelopment Authority. It is currently in use as a public car park and any future development must maintain this use. The site should be developed as a commercial building and has been identified as a potential market site. Any development at the site is to have an active frontage that will maintain the continuous built edge along Roberts Road. If the Subiaco Markets or other commercial uses are located on the site they are to be at ground level with active frontages to and connection between Roberts Road and Vickers Lane. Carparking remaining at the site is to be screened from public areas and access to car parking will not be permitted to impact on the ground floor uses of the site. The lot is currently used for a public car park, any future development must maintain the current number of bays on the site, plus any additional bays as required by future uses. The lot is located in the Subiaco Tunnel Zone of Influence; see Section 4.9 for details. Sewer easement located on north of lot. The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.					
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land use/s	Vehicular Access
Lot 1 (No. 29) Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Nil permitted	3.0:1	Commercial Residential	Only via existing rear entry off Vickers Lane
Development Intent: Mixed use commercial and residential building with activated frontage to Hood Street and the intended pedestrian connection shown on the lot plan.					
Other Design Requirements: The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.					

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Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Uses	Vehicular Access
Lot 1 (No. 31) Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Nil permitted	3.0:1	Commercial Residential	Only via existing rear entry off Vickers Lane
Development Intent: Mixed use commercial and residential building with activated frontage to Hood Street and the intended pedestrian connection shown on the lot plan.					
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Uses	Vehicular Access
Lot 10 Roydhouse Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 Storeys Roydhouse Street: Nil - Min 4m above 4 Storeys Sides: Nil permitted Rear: Min 7.5m from rear boundary plus Min 4m above 4 storeys as shown on lot diagram	3.0:1	Commercial Residential	Hood Street Roydhouse Street
Development Intent: Mixed use commercial and residential buildings that improve the permeability of the Precinct by providing a connection between Roydhouse and Hood Streets. Development is to be active commercial uses to the ground floor, and is to address the intended pedestrian connection through the lot.					
Other Design Requirements: Development proposals are to provide for a street connection between Roydhouse and Hood Streets. Finished ground floor levels are to be within 100mm of existing or new street levels to ensure good interface between ground floor uses and the streetscape. The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.					
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Uses	Vehicular Access
Lot 33 Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood street: Nil - Min 4m above 4 Storeys Side: Nil permitted Rear: Min 7.5m plus 4m above 4 storeys	3.0:1	Commercial Residential	Hood Street
Development Intent: Mixed use commercial and residential building with activated frontage to Hood Street.					
Other Design Requirements: The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.					

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Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 32 Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Min 7.5m plus minimum 4m above 4 storeys	3.0:1	Commercial Residential	Hood Street
Development Intent: Mixed use commercial and residential building with activated commercial frontage to Hood Street. Lot 32 has been identified as a location of an intended pedestrian connection that would improve permeability in the Precinct. Proposals that provide this connection are encouraged, see section 2.2 for more information.					
Other Design Requirements:	The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.				
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 31 Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Min 7.5m plus minimum 4m above 4 storeys	3.0:1	Commercial Residential	Hood Street
Development Intent: Mixed use commercial and residential building with activated commercial frontage to Hood Street.					
Other Design Requirements:	The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.				
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land use/s	Vehicular Access
Lot 30 Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Min 7.5m plus minimum 4m above 4 storeys	3.0:1	Commercial Residential	Hood Street
Development Intent: Mixed use commercial and residential building with activated commercial frontage to Hood Street.					
Other Design Requirements:	The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.				

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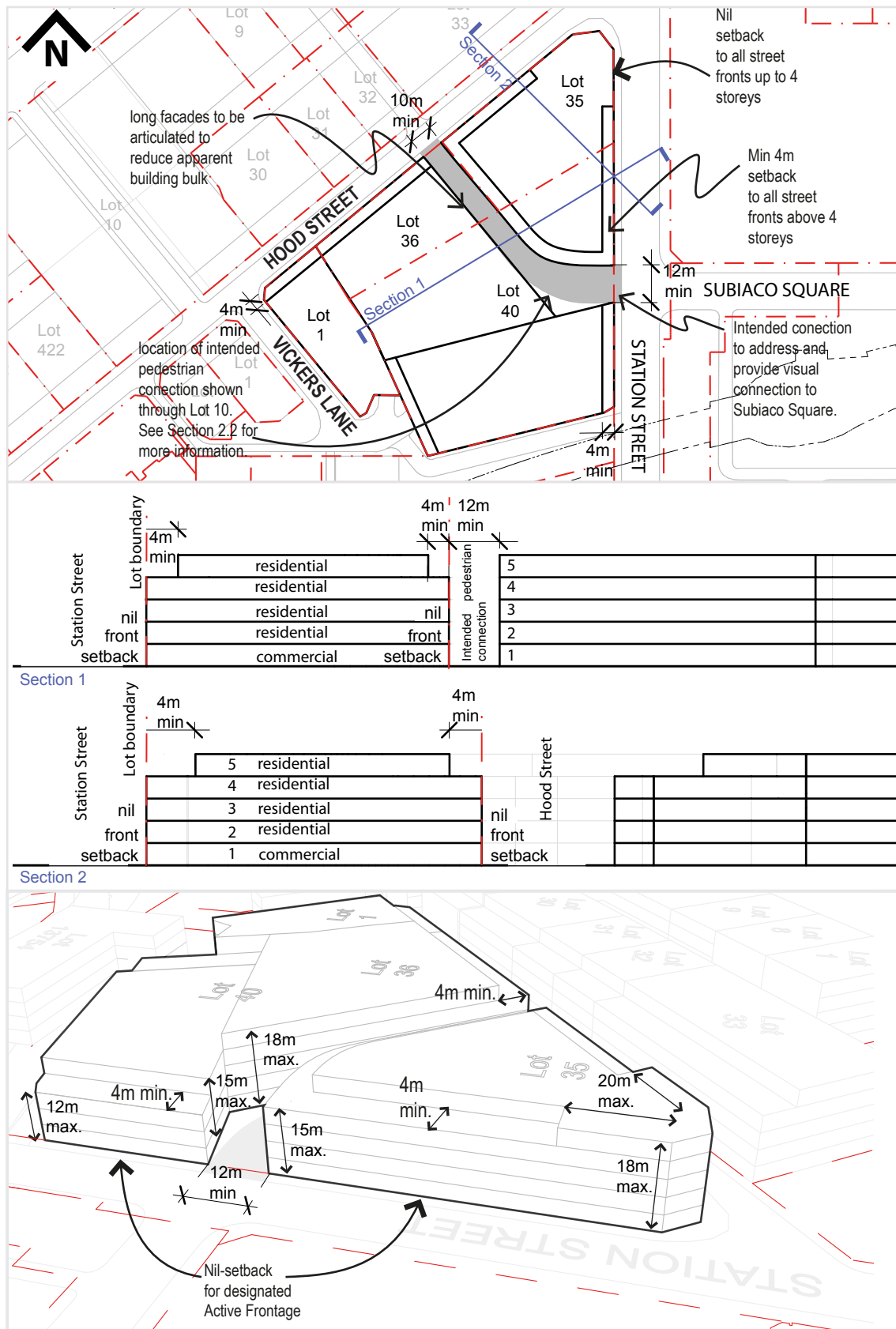
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 9 Roydhouse Street	4 storeys up to 15m 5 storeys up to 18m	Roydhouse Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Min 7.5m, plus minimum 4m above 4 storeys	3.0:1	Commercial Residential	Roydhouse Street
Development Intent: Mixed use commercial and residential building with activated commercial frontage to Roydhouse Street.		Other Design Requirements: The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.			
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 8 Roydhouse Street	4 storeys up to 15m 5 storeys up to 18m	Roydhouse Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Min 7.5m, plus minimum 4m above 4 storeys	3.0:1	Commercial Residential	Roydhouse Street
Development Intent: Mixed use commercial and residential building with activated commercial frontage to Roydhouse Street. Lot 8 has been identified as a location of an intended pedestrian connection that would improve permeability in the Precinct. Proposals that provide this connection are encouraged, see section 2.2 for more information.		Other Design Requirements: The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.			
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 1 Roydhouse Street	4 storeys up to 15m 5 storeys up to 18m	Roydhouse Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Min 7.5m plus minimum 4m above 4 storeys	3.0:1	Commercial Residential	Roydhouse Street
Development Intent: Mixed use commercial and residential building with activated frontage to Roydhouse Street.		Other Design Requirements: The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1-4 of the design guidelines.			

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Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 422 Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Min 7.5m plus minimum 4m above 4 storeys	3.0:1	Commercial Residential	Hood Street
Development Intent: Mixed use commercial and residential building with activated commercial frontage to Hood Street.		Other Design Requirements: The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.			
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 28 Hood Street	4 storeys up to 15m 5 storeys up to 18m	Hood Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Min 7.5m plus minimum 4m above 4 storeys	3.0:1	Commercial Residential	Hood Street
Development Intent: Mixed use commercial and residential building with activated commercial frontage to Hood Street.		Other Design Requirements: The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.			
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 12 Roydhouse Street	4 storeys up to 15m 5 storeys up to 18m	Roydhouse Street: Nil - Min 4m above 4 storeys Side: Nil permitted Rear: Min 7.5m, plus minimum 4m above 4 storeys	3.0:1	Commercial Residential	Roydhouse Street
Development Intent: Mixed use commercial and residential building with activated commercial frontage to Roydhouse Street.		Other Design Requirements: The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.			
Lot No.	Maximum Height	Setbacks	Max Plot Ratio	Preferred Land Use/s	Vehicular Access
Lot 11 Roydhouse Street	4 storeys up to 15m 5 storeys up to 18m	Roydhouse Street: Min 4m above 4 storeys Side: Nil permitted Rear: Min 7.5m, plus minimum 4m above 4 storeys	3.0:1	Commercial Residential	Roydhouse Street
Development Intent: Mixed use commercial and residential building with activated commercial frontage to Roydhouse Street.		Other Design Requirements: The building parameters detailed in this table represent the maximum building envelope only, and are not as of right. All developments must also comply with the provisions of Part 1 - 4 of the design guidelines.			

5.7 Lot Plan and Building Envelope Diagrams (Part 1)

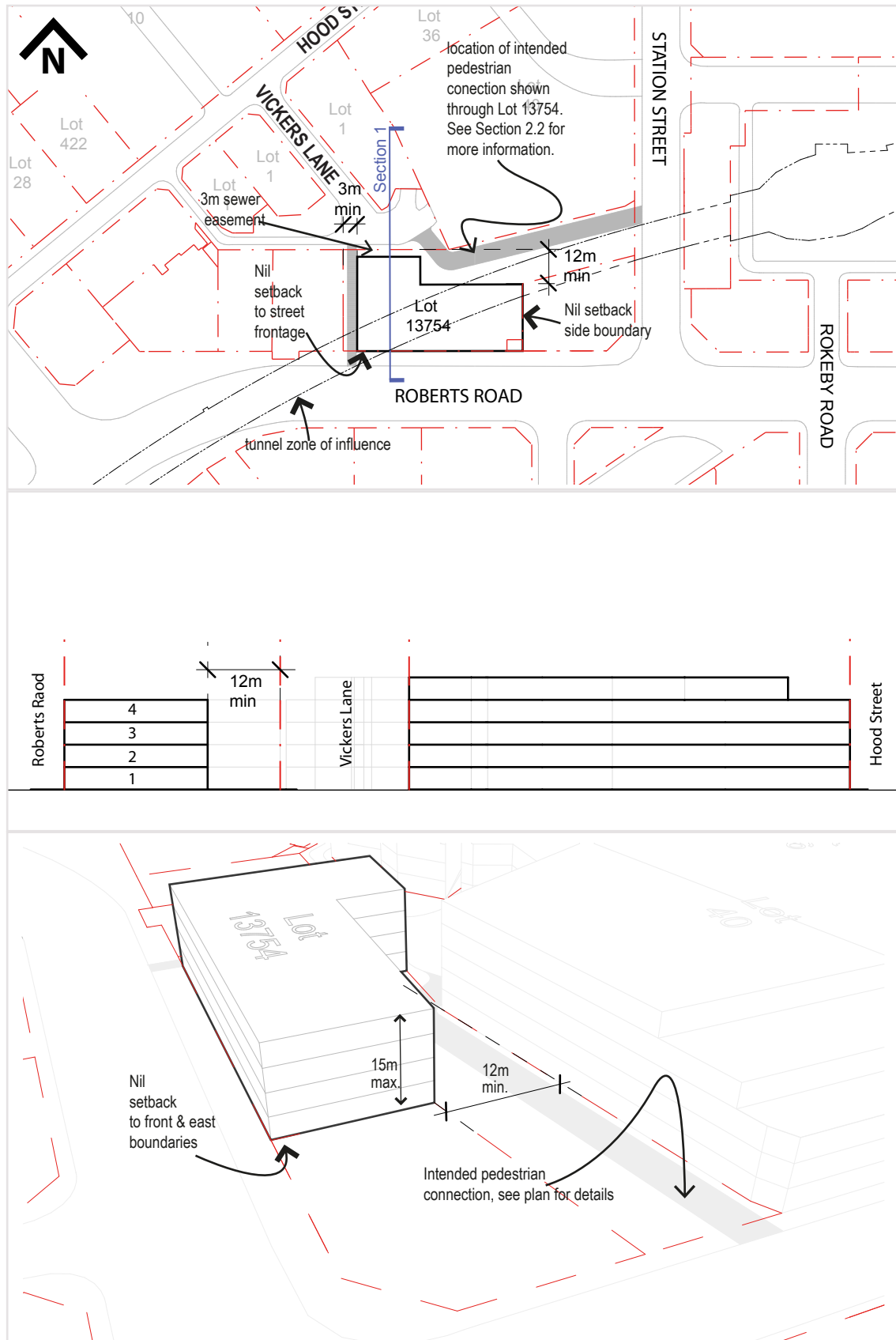
Lots 1, 35 and 36 Hood Street and Lot 40 Station Street



Note: The information shown on these diagrams, such as lot boundaries, lot sizes and the Tunnel Zone of Influence, is indicative only and subject to change. Please refer to the relevant Certificates of Title for the most up-to-date information.

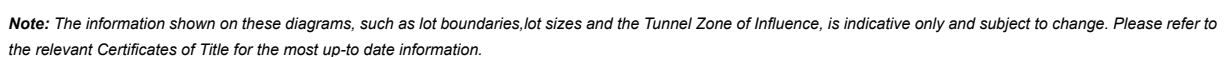
5.8 Lot Plan and Building Envelope Diagrams (Part 2)

Lot 13754 Roberts Road



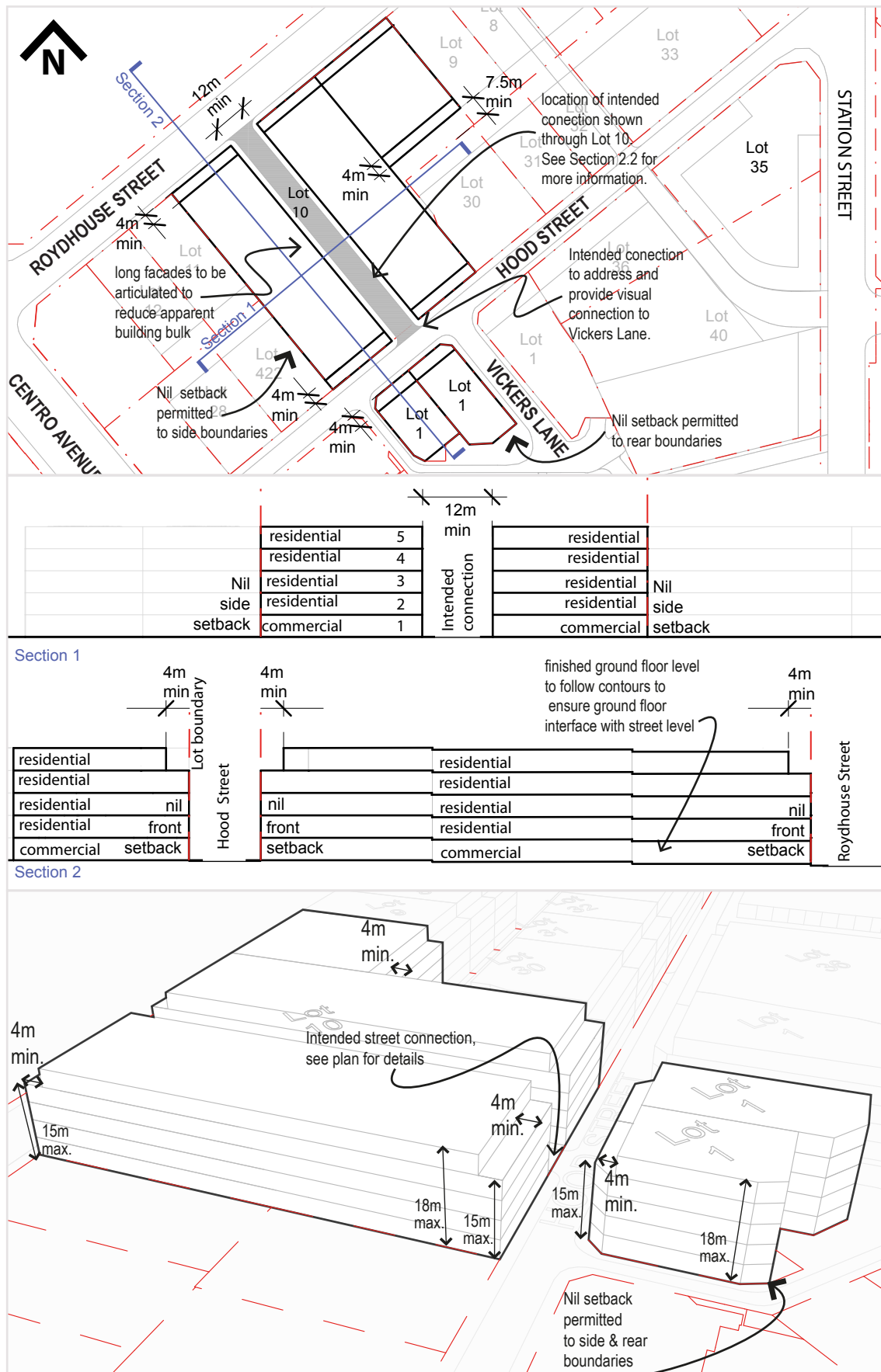
Note: The information shown on these diagrams, such as lot boundaries, lot sizes and the Tunnel Zone of Influence, is indicative only and subject to change. Please refer to the relevant Certificates of Title for the most up-to-date information.

Lots 1, 8 and 9 Roydhouse Street and Lots 30,31,32 and 33 Hood Street



5.10 Lot Plan and Building Envelope Diagrams (Part 4)

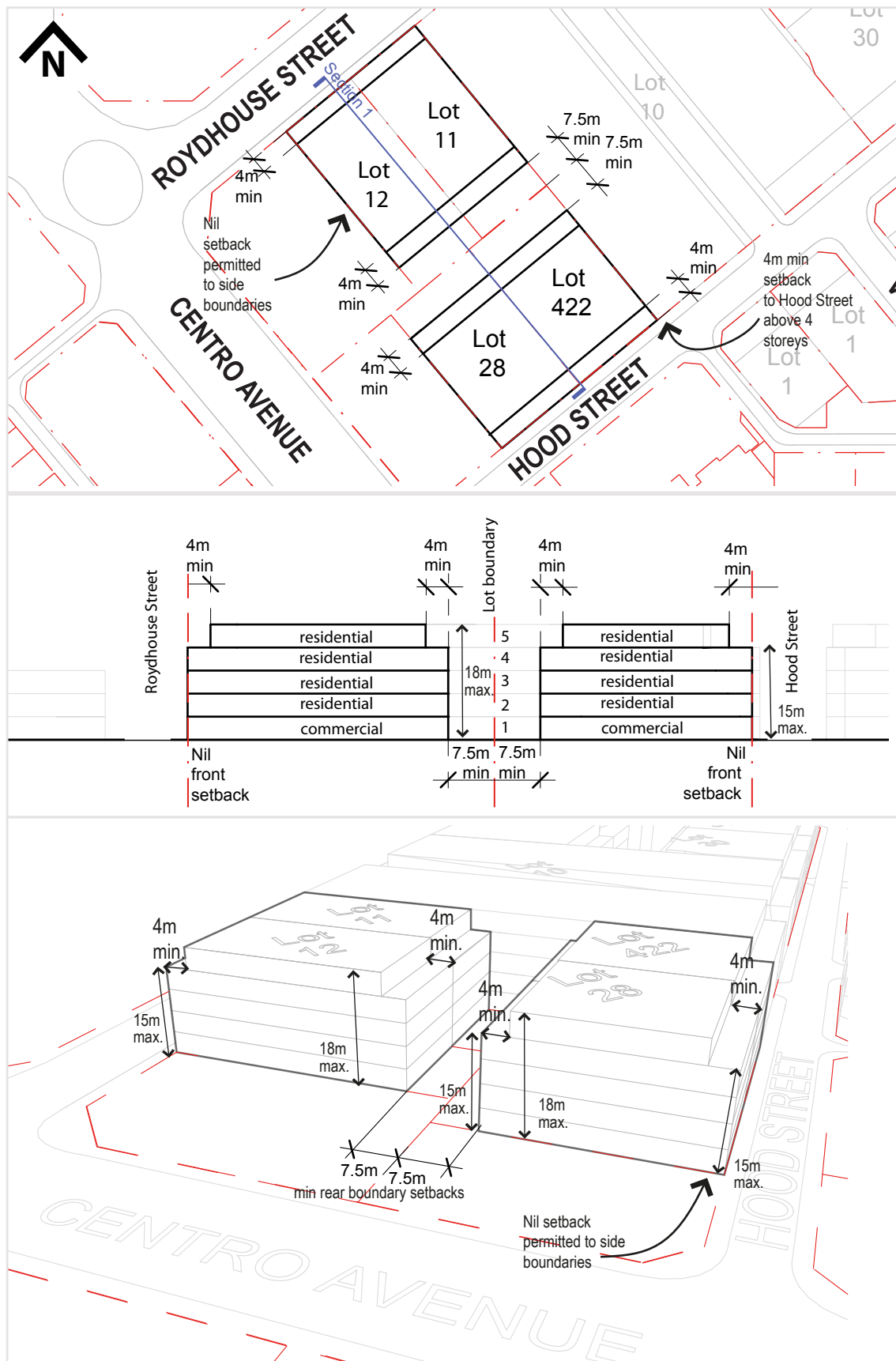
Lots 1 (No 29) and 1 (No 31) Hood Street and Lot 10 Roydhouse Street



Note: The information shown on these diagrams, such as lot boundaries, lot sizes and the Tunnel Zone of Influence, is indicative only and subject to change. Please refer to the relevant Certificates of Title for the most up-to-date information.

5.11 Lot Plan and Building Envelope Diagrams (Part 5)

Lots 11 and 12 Roydhouse Street and Lots 422 and 28 Hood Street



Note: The information shown on these diagrams, such as lot boundaries, lot sizes and the Tunnel Zone of Influence, is indicative only and subject to change. Please refer to the relevant Certificates of Title for the most up-to-date information.



DOCUMENT CONTROL

Hood Street Design Guidelines

Date of Adoption: 17 November 2011

Record of Amendments

Section / Clause	Nature of Amendment	Adoption Date

