

# Centro North



## DESIGN GUIDELINES



# CONTENTS

Introduction	3
Precinct plan and context	4
Relationship to redevelopment scheme and policies	6
Design development and approval process	7
General provisions	
1.0    land use	8
2.0    built form	9
3.0    building design	14
4.0    sustainable design	17
5.0    other provisions	19
Lot specific provisions	
lot 1	21
lot 2	26
lot 3	28



The Centro North Design Guidelines and Performance Standards for Development have been prepared by the Subiaco Redevelopment Authority (Authority) for land at the former Subiaco TAFE site, to guide the continuing redevelopment of Subiaco. Centro North endeavours to forge a link between the retail core surrounding the train station and St John of God Hospital, with an emphasis on the village way of life, in order to create a cohesive, modern and vibrant cosmopolitan community.



Formulation of the guidelines involved consultation with state and local government representatives as well as other professional consultants (i.e. heritage architects, property consultants and engineers) to provide direction for appropriate development forms, whilst taking into account the various built form and social qualities associated with Subiaco. The outcomes from the preparation and implementation of the guidelines have resulted in a mixture of compatible land uses such as residential, retail and commercial, and a minimisation of any adverse effect on residential amenity by developing appropriate design assessment criteria and applying specific mitigation requirements.



*The site in context*



## PRECINCT PLAN AND CONTEXT

The former TAFE site at Station Street, Subiaco, now known as the Centro North precinct, represents an opportunity to bolster Subi Centro as a vibrant mixed use precinct with good access to public transport. Opportunities exist for retail and commercial development to build on the existing retail core as well as capitalise on the site's position adjacent to St John of God Hospital. High density, urban living is promoted, making for a convivial, safe and attractive precinct. The site is also adjacent to the state heritage listed St Josephs Church.

The Centro North precinct should provide attractive, pedestrian-oriented streetscapes which adequately accommodate vehicles and which will draw people to an activated Urban Plaza. The precinct should contain interesting outward focused buildings that engage those who live, work and visit through the use of innovative building design and materials. In this regard, buildings can engage with the street and pedestrians, making for a comfortable urban environment.

With good opportunities for ground floor retail and commercial development, it is envisaged that above the ground floor, the precinct will be dominated by apartment-style dwellings of various sizes with generous balconies overlooking the streets and Urban Plaza to encourage social activity and passive surveillance of the public realm.

Distinctive buildings contribute to legibility within the precinct and provide an enhanced sense of place. The buildings will engage with the Urban Plaza via appropriate openings and ground floor uses that create a transition between private and public realm. These uses will 'spill out' onto the plaza, creating life and interest for passing pedestrians and a point of identity for the precinct. A secondary Urban Green will be established at the north western extent of the site. This green space will allow for the retention of established trees, thus providing amenity for residents and users of the area. Ground floor commercial uses will address the Urban Green with upper floor residential uses providing passive surveillance.

Vehicle parking will be located in basements with some on-street parking to further enhance the pedestrian-oriented nature of the precinct. Overall, the precinct will form a dense urban environment that takes advantage of its close proximity and good access to public transport.

The provisions of the guidelines are divided into 'general' and 'lot specific'. The 'general' provisions apply to all development within the Centro North precinct, unless varied by a provision within the relevant 'lot specific' provisions. If there is a perceived or apparent discrepancy between these general provisions and the provisions applicable to a lot, the 'lot specific' provisions will apply.



\_\_\_\_\_



These Design Guidelines are adopted under the provisions of Part 5 of the Subiaco Redevelopment Scheme (the Scheme) and to the extent of the Centro North Precinct, supersede the Subi Centro Station Precinct Design Guidelines.

General planning policies applicable to the Redevelopment Area are also outlined in the Scheme. These guidelines have been adopted by the Subiaco Redevelopment Authority (Authority) as policy in accordance with Scheme provisions, and should be read in conjunction with the Scheme and other planning policies. In determining any application for development approval, the Authority will utilise these guidelines, in conjunction with the Scheme and policies, as the primary assessment criteria.

## DESIGN DEVELOPMENT AND APPROVAL PROCESS

The Authority aims to ensure all land owners and their architects are aware of the Authority's design objectives and have formulated a process to achieve the best possible design through communication outcomes.

- Step 1 Subiaco Redevelopment Authority (Authority) sells land (tender / auction / private treaty)
- Step 2 The Authority meets with developer and their project team (architects at a minimum) to discuss design concepts
- Step 3 Developer provides the Authority with indicative plans
- Step 4 The Authority provides developer with focused feedback
- Step 5 Lodge Development Application with the Authority
- Step 6 Referral to City of Subiaco, Town of Cambridge and other government agencies (such as the Heritage Council, as required)
- Step 7 Review of application by the Authority's Design Review Panel
- Step 8 The Authority determines the Development Application
- Step 9 Lodge Working Drawings with the Authority demonstrating compliance with Development Approval (plans and conditions)
- Step 10 Lodge Building Licence with City of Subiaco (separate application process)
- Step 11 The Authority issues cleared Working Drawings
- Step 12 The City of Subiaco issues Building Licence
- Step 13 Construction and Compliance

# GENERAL PROVISIONS

## 1.0 LAND USE

### scheme and specific lot provisions

The Subiaco Redevelopment Scheme provisions apply. However, refer to the ‘lot specific’ provisions to determine appropriate mixture of uses and specifically the required ground floor uses.

*In some instances, ground floor uses in the form of shops, cafes, restaurants and similar uses are desired to provide activity.*



### residential dwelling types

Each new development must incorporate a mixture of one-bedroom, two-bedroom and three-bedroom units in accordance with the Authority’s Housing Diversity Policy. This will ensure a mix of dwelling types across the precinct, whilst allowing for a greater proportion of some types in some areas, according to market demand and the requirements within each Precinct.



### mixed-use

Mixed use may include housing above commercial or retail uses, or housing next to and integrated with other permitted uses.

Uses should be given separation to maintain amenity and privacy for residences. Generally separation by level is required, however where this cannot be achieved, separation is required through the provision of access points and dividing walls.

### floor areas

Minimum floor areas apply to dwelling types as follows:

Dwelling Type	Minimum Floor Area
Studio	40sqm
1 Bedroom	50sqm
2 Bedrooms	75sqm
3+ Bedrooms	90sqm

*Floor area is net floor area including all habitable and non-habitable rooms and is exclusive of balconies.*

*A Studio is a dwelling that comprises living and sleeping areas contained within one room and where there is no separate bedroom.*

### densities

Maximum residential densities are to comply with the provisions of the Subiaco Redevelopment Scheme for the Centro North Precinct.

A minimum dwelling yield of no less than 10% below the maximum yield should be applied to each lot unless otherwise approved by the Authority. This is to achieve maximum benefit from the location of Centro North in relation to adjacent Subiaco Train Station.



### lot diagrams and building envelopes

The following required elements are illustrated on the Lot Diagrams:

- Building Setbacks
- Maximum Building Heights
- Vehicle Access Points
- Active Ground Floor use locations

### building setbacks

The first two levels above ground are required to be developed to the extent of the building envelope for 70% of the lot frontage. 30% of the frontage may have a setback of up to 4m from the building envelope to allow for building articulation and any residential courtyards. Buildings may be set back from the building envelope above the second storey.

*A nil setback applied to the ground and first floors will create a streetscape with a defined built form.*

### building height

Habitable floor space of buildings shall not exceed the maximum height specified on the attached relevant Lot Diagram and identified within the 'lot specific' provisions. Roof plant, lift over-runs and roof terraces may be contained above the maximum height to a maximum of 4 metres and subject to no visual intrusion to the streetscape.

*Building height is to be measured vertically from the ground level at each corner of a lot boundary. Building height is measured in metres and expressed as storeys.*

Under-croft and basement car parking is not considered to be a storey but any part of a building used for parking above ground level is included in the calculation of overall building height.

*Parking above ground level would be considered to constitute a storey and is therefore not encouraged.*

All Buildings within the Centro North Precinct are to be a minimum of 2 storeys in height.

# GENERAL PROVISIONS

## 2.0 BUILT FORM

### solar orientation

Direct natural lighting should be provided to all living, dining and sleeping areas of each dwelling (not limited to the BCA minimum requirements). However where reliance on borrowed light is demonstrated to be unavoidable, only non-habitable rooms and kitchens shall be designed with access to borrowed light.

*Design requiring borrowed light results in darkened internal environments, leading to dependence on artificial lighting and greater energy use, which should be avoided.*

### site cover

Site cover is determined by the setbacks relating to each lot, requirement for open space and adequate access to natural light and ventilation and includes undercroft and enclosed car parking areas but excludes open air areas and on-grade car parking.

### plot ratio

Plot ratio is as specified in the Scheme.

### private and communal open space

Private open space must be provided for each dwelling that is of a usable size (10sqm for balconies and 12sqm for ground floor courtyards) and dimensions (3m). Consideration will be given to smaller areas and dimensions but this must be justified to the satisfaction of the Authority.

Private open space, particularly upper floor balconies, are to be sensitively located or screened to avoid any potential visual privacy impact between dwellings within the lot or on adjoining lots. If screening is necessary, it should be integrated into the building design and must not unduly add to the building bulk.

Balconies shall not extend into the existing or proposed road reserve.

*It is not possible to create titles for private balconies that extend into public space.*

If communal open space is provided within the development, then its location and the facilities provided within it should be such that they will not be the source of noise or other nuisance for occupants of the development or those on immediately adjoining neighbouring properties.





### safer design

The design of buildings should allow passive surveillance of communal areas and public spaces, and each dwelling should have at least one major opening from a habitable room or balcony overlooking the street, public space or communal open space.

Loading and storage areas should be well lit and lockable after hours.

Pedestrian entrances are to be highly visible and identifiable as entry points, within a 180 degree line-of-site, from each entry point. Minor obstructions to views are acceptable.

*The ability to see to building entries from the street, and vice versa, enhances safety and assists in way-finding.*

Ground floor non-residential frontages should be designed as shop fronts with no less than 70% of the shop front glazed with clear glass.

*Glazed "shop fronts" with clear glass create an interesting pedestrian environment and enhance safety through passive surveillance.*



### levels

Ground floor levels of residential buildings are to be at or near ground level and in any case no more than 1.2m above pavement level.

*Retail and commercial development should generally be at ground level.*

Where a basement wall extends more than 1m above ground level, it shall be treated with landscaping boxes or similar in order to soften the edge of the building.

It is acknowledged that where residential uses are proposed for the ground floor, a small level change between the private and public realm can aid in the privacy and liveability of dwellings. The interface of any level change should be consistent with the provision above as well as allowing for passive surveillance of the street via permeable fencing.

# GENERAL PROVISIONS

## 2.0 BUILT FORM

### vehicles and parking

Vehicle access to be provided at a single point for each lot within the area specified on the relevant Lot Diagram.

*Vehicle crossovers to streets impacts negatively on the streetscape and pedestrian environment and reduces the potential for on-street car parking.*

Refer to Table 1 – Car Parking Requirements and Clause 36 of the Scheme for parking requirements for non-residential uses.

*Parking for non residential uses in the Precinct takes into account the proximity to the Subiaco train station and other public transport nodes.*

Car parking bays are to be provided at an average maximum of 1.4 bays per dwelling for each lot. This average is to be achieved as follows:

Dwelling Size	Minimum No. Bays per Dwelling	Maximum No. Bays per Dwelling
Up to 60sqm floor area	1	1
Over 60sqm floor area	1	2

*The maximum number of bays for any dwelling is 2 because of the proximity of public transport and sustainability of design.*

On-site visitor car parking is not mandatory for residential development, including residential within mixed use developments.

Short term parking for delivery and service vehicles should preferably be provided on-street, in basement car parking areas or behind the built form.

*Short term parking may be provided within setback areas if no reasonable alternative exists, in the opinion of the planning authority.*

Reciprocal use of car parking bays for uses within a comprehensive development with different peak usage requirements may be approved, provided that bays provided for residential use are available at all times.

Car parking for dwellings will not be permitted to be visible from the street and is to be provided as a basement or concealed behind the built form

*This provision aims to reduce the visual impact of parking located at the front of buildings and the impact on the pedestrian environment.*

Traffic Impact Statements/Assessments are required to be submitted with each Development Application to ensure that traffic for each can be appropriately accommodated and managed.





### visual truncations for vehicle access

A visual truncation of 2m x 2m on the exiting driver's side is required for vehicle exits on site boundaries or on both sides adjacent to a pedestrian path or other vehicle route.

*Safety for vehicles and pedestrians is to be achieved by ensuring that critical sight lines are unobstructed. A visual truncation may be three-dimensional, that is, it is not required for the full height of the building, and low objects may be possible within the truncation in some situations.*

### bicycle parking

Provision shall be made for the parking of visitor bicycles for non-residential uses at a rate of 1 bicycle parking bay per 200sqm of Net Lettable Area (NLA). End-of-trip facilities including showers space are also required to be provided and can be shared or provided within individual tenancies.

*This could be simple rails attached to walls near building entries, where bikes can be chained out of the way of pedestrian traffic. The City should also ensure that there are bicycle hitching rails within the street, particularly in the retail and commercial nodes.*

Bicycle parking for non-residential uses is to be conveniently located and designed in accordance with Australian Standard 2890.3 to provide ease of access for bicycle users.

Bicycle parking for residential uses shall be in the form of appropriately sized store rooms.

### universal access

Each new development must provide a proportion of accessible dwellings in accordance with the Authority's Accessible and Adaptable Housing Policy.

*The Disability Discrimination Act and the relevant Australian Standard should also be consulted to determine the requirements for universal access.*



# GENERAL PROVISIONS

## 3.0 BUILDING DESIGN

### architectural style and design

Building bulk and building scale shall relate to neighbouring buildings or known future buildings.

*Contemporary architectural expression is not precluded by this requirement.*

Sufficient variation in building plane, colours and texture must be provided so as to reduce the overall bulk and form of any development.

Building design should demonstrate an understanding and interpretation of context and may incorporate modern forms and materials but should do so through understanding and reinterpretation of fundamental principles of the surrounding urban form.

Development is to be articulated in order to continue the typical rhythm of building form within Subiaco. Development should incorporate areas of vertical and horizontal articulation in order to achieve this design principle.

The floor to floor height of all ground floor development must be a minimum of 3.5m above the finished height of the adjacent footpath to ensure that buildings have a visually consistent first floor alignment externally. This will also provide a robust design in order to allow flexibility of uses at the ground level.

### roof form

Roof forms should be of a contemporary nature. A mix of pitched, flat, skillion or curved roofs can be supported, consistent with the intended architecture of the precinct.





### facades

Facades must be articulated and there shall be no extensive blank walls as these can become a target for vandalism and prevent passive surveillance of adjacent spaces, reducing safety.

Basement level car parks are to be architecturally treated to ensure the protection of the visual amenity of the development site and adjoining area. Large expanses of exhaust grills must be avoided.

Building facades should be articulated through the considered placement of windows, entrances, balconies, and awnings.

*Well articulated buildings are sought, to add interest and visual cohesiveness to the streetscape.*

At a minimum, any exposed parapet wall must have the same level of finish as the primary facade as unfinished blank walls are unacceptable.

Detailing for permanently exposed blank walls could include textured patterning or art such as mosaics or murals. Durability and maintenance should be considered.

All buildings must address adjacent streets, utilising major windows, pedestrian entrances, balconies and other similar elements to enrich and activate the streetscape.

*This applies to the primary street frontage, as well as rear access ways and promotes “eyes on the street” or passive surveillance.*

External ducting, air conditioners, plant, pipes, lift over-runs, service doors and similar building services must be screened from view from any public place or adjacent property and incorporated into the building at the initial design stage.

Mixed use buildings should have separate entries for non-residential and residential uses to provide clarity for pedestrians and visitors to the development. Residential lobbies are to be clearly delineated, well lit and safe to access.

Residential entries should not be located on the north-south axis of the internal street to ensure that this area contains activated “shop-fronts”. Residential entries may however be supported on the corners of the north-south axis of the internal street.

*Residential areas are considered to provide a degree of activation.*

# GENERAL PROVISIONS

## 3.0 BUILDING DESIGN

### materials

The selection of building materials and external finishes is to be informed by the palette used in the surrounding area. The palette of colours and materials employed on the exterior of a building is to generally achieve some contrast, with minimal use of highly reflective or gloss materials or colours.

*An appropriate mix of modern and traditional building materials is encouraged.*

Extensive use of tilt-up/panel concrete is discouraged where it detracts from the intended character and amenity of the precinct.

The street level should contain a high degree of materiality at the human scale to provide visual interest and delight for pedestrians



### ground floor tenancy depth

Tenancies fronting Station Street, Salvado Road, the north-south axis of the internal road and the Urban Plaza are to have a minimum depth of 10m as shown on the relative Lot Diagram

*A minimum depth is specified to ensure that retail and commercial tenancies are robust and adaptable.*



### pedestrian amenity

Where possible, a continuous pedestrian shelter in the form of a verandah or awning is to be integrated into the design of buildings facing Station Street and Salvado Road. Buildings on lots 2 and 3 adjacent to the north-south axis of the internal road shall provide pedestrian shelter at key locations at the intersection of Wexford Street, the 90-degree turn and to the Urban Plaza as shown on the relevant Lot Diagram. The pedestrian shelter should extend across a minimum of 80% of each frontage. Pedestrian shelter designs should also take into account any street trees to allow for canopy spread and ongoing maintenance.

Awning heights should be consistent, relative to topography height to reinforce the visual connection of buildings along the street. Awnings may be raised at entrance ways or lobbies to identify an entry point.



### energy efficiency

Buildings are to be designed to help minimise operational energy consumption and greenhouse emissions.

Tenancy sub-metering and energy demand reduction measures are to be included in new mixed-use and multiple dwelling residential developments.

All mechanical devices and appliances installed as part of the development including air conditioners, heating devices and clothes dryers are to have a minimum 5-star energy rating from the Department of the Environment and Water Resources Australian Greenhouse Office. Solar or gas hot water systems are encouraged rather than electric hot water systems.

Proposals for new development should be accompanied by a report prepared by a suitably qualified sustainability consultant, outlining the design and management elements (including financial and environmental costs of operating buildings after construction) proposed to be implemented to optimise sustainability performance.

*It is acknowledged that there are many ways of addressing energy efficiency and sustainability within developments, and that innovations are continually being introduced.*

Dwellings must be designed and constructed to a minimum energy efficiency rating of 5 stars as measured against a rating system that complies with the Australian Building Code Board Protocol for Housing Energy Rating Software. The qualified sustainability consultant should identify the energy efficiency rating of dwellings within the required report.

The heat loading effect of the summer sun on west and north facing windows should be minimised. Techniques such as external sun shading devices should be considered and dark or reflective tinting should be avoided.

*External sun shading is more efficient at reducing the heat load on windows and also provides extra architectural detail and interest to the façade.*

Buildings are to achieve a high level of cross-ventilation through the placement of openings, therefore reducing the need for air-conditioning.

# GENERAL PROVISIONS

## 4.0 SUSTAINABLE DESIGN

### water consumption

Buildings are to be designed to reduce water consumption by occupants through such measures as sub-metering of water use, alternatives to water based building cooling systems, grey water usage, rainwater capture and re-use, water efficient fittings and fixtures (e.g. taps, toilets) and water sensitive landscaping design.

### gardens and landscaping

Landscaping should incorporate native and low water use plant species.

*Water-wise planting is strongly encouraged in the precinct.*

### waste management

Use of locally available materials and recycled materials and minimal or no use of environmentally harmful materials is strongly encouraged.

*The embodied energy and environmental impacts of transport should be considered when selecting materials for development*

Provision is to be made within each development for adequately sized bin storage areas to accommodate the separation of recyclable waste.







### fences

Any fencing fronting onto a street development is not to exceed 1.2m in height. If a fence is proposed higher than 1.2m, that portion of the fence above 900mm will be 70% visually permeable.

No at-grade boundary fence may exceed 1.8m in height above finished footpath level.

Fences above basement or retaining walls more than 800mm above footpath level shall not extend more than 1m above the finished floor level. Any component of the fence more than 1.2m above the footpath shall be 70% visually permeable.

Letter boxes should be incorporated into the fence or into the development.

### noise

Buildings are to be designed and constructed to appropriately deal with sound intrusions such as traffic, noise from the Urban Plaza and the adjoining St Josephs Church, in accordance with the Authority's Acoustic Attenuation Policy.

### bin and service enclosures

Waste and recycle bins and other areas dedicated for services are to be integrally designed into the buildings and conveniently accessible to occupants and such areas must be screened from the street to the satisfaction of the planning authority.

# GENERAL PROVISIONS

## 5.0 OTHER PROVISIONS

### storage

A storage room with a minimum floor area of 5sqm to allow for bicycle storage is to be provided for each dwelling unit and must be integrally designed into the buildings, external to the associated dwelling, conveniently accessible to occupants and ideally located within basements.

### signage

All signage is to comply with the Authority's Signage Policy.

Signage should be designed as an integral part of the building design and should not appear as a later attachment.

### public art

Public art is to be integrated into the development of each lot either within the building or as a monetary contribution in accordance with the Authority's Public Art Policy.

### other services

The provision of outdoor clothes drying areas are encouraged as a way of minimising the use of clothes dryers (and thereby reducing energy consumption) in the precinct, and should be incorporated into developments wherever possible. Outdoor clothes drying areas are required to be screened from view from the street.

### stormwater

All stormwater shall be contained on site. Storm water retention must be indicated on the plans submitted at working drawing stage.





### character statement

Lot 1 presents an opportunity for mixed use, residential, retail and commercial development with retail and commercial uses fronting the Urban Plaza, Urban Green, Salvado Road and Station Street. Development of Lot 1 should be up to 4 storeys in height and incorporate 3 distinct façade elements containing varied materials, setbacks and fenestration to promote architectural interest and break up the building bulk.

Sociable uses such as cafes and restaurants are encouraged to locate adjacent to the Urban Plaza to promote the emergence of an engaging and active place. Retail and commercial uses fronting the Urban Green, Salvado Road and Station Street may relate to the hospital on the northern side of Salvado Road and may compliment commercial development on the western side of Station Street.

A significant and identifiable building should be developed on the corner of Salvado Road and Station Street to act as a landmark for the precinct and should appropriately address both streets. The corner building may incorporate additional height to further articulate the corner. Vehicle and service access should be located on the internal road which will ensure a safe and attractive pedestrian environment.

These provisions apply to all development on Lot 1, as indicated on the relevant Lot Diagram. Where there is an apparent discrepancy between these provisions and the general provisions, the lot specific provisions will apply.

### land use

Land uses for Lot 1 are to be in accordance with the Scheme. Land uses for Lot 1 on the ground floor facing Salvado Road and Station Street are to include commercial and retail uses such as Offices, Medical Centres, Consulting Rooms and Shops, while land uses such as Local Shops and Restaurants/cafes should be concentrated along the Urban Plaza to encourage activation. Community uses such as Educational Establishments and Day Care Centres may also be developed on Lot 1. The ground floor and upper floors facing the internal road should contain residential uses or Serviced Apartments.

Residential uses are not permitted on the ground floor fronting Station Street, Salvado Road or the Urban Plaza.

# LOT SPECIFIC PROVISIONS

## LOT 1

### building articulation

The building envelope for Lot 1 has been formulated in order to achieve 3 distinct building elements. Whilst the architectural style of the development may continue throughout the building form, it must exhibit a range of set-backs, materials and fenestration in order to create visual interest and help to break down the bulk of the building.

### roof form

The roof form of development on Lot 1 should be broken into distinct elements and not appear as one large element.

### building height

An additional storey to provide a corner treatment to the Salvado Road/ Station Street intersection may be supported subject to appropriate design. This additional storey should not comprise more than 60sqm of floor area and be integrated into the building rather than appearing as a separate element. The corner treatment should complement the adjacent Urban Green, whilst being of visual interest.





#### relationship to urban plaza

The ground floor use of Lot 1 where it abuts the Urban Plaza is to be an active non-residential use, which will provide an interface with the Urban Plaza through the use of glazed windows and a generously sized entrance way.

*This could be achieved through the use of concertina or café-style bi-fold doors.*

All levels on the eastern side of Lot 1 are to address the Urban Plaza through the use of windows to active habitable rooms and useable balconies.

The built form should be built with a nil setback or as close to the boundary as possible, with a maximum setback from the eastern boundary of 1.5m.

*A nil setback may not be achievable to the lot interface with the urban plaza given the irregular nature of the lot.*

The floor level of the building adjacent to the urban plaza shall be such that at grade access can be equitably achieved.

Any exposed external setback area between the building and eastern boundary should be paved with materials consistent with the Urban Plaza.



# LOT SPECIFIC PROVISIONS

## LOT 1

### relationship to urban green

The ground floor use of Lot 1 where it abuts the Urban Green should be an active non-residential use, which will provide an interface with the Urban Plaza through the use of glazed windows and a generously sized entrance way. The use may be a shop, cafe, consulting suite, office or similar activity.

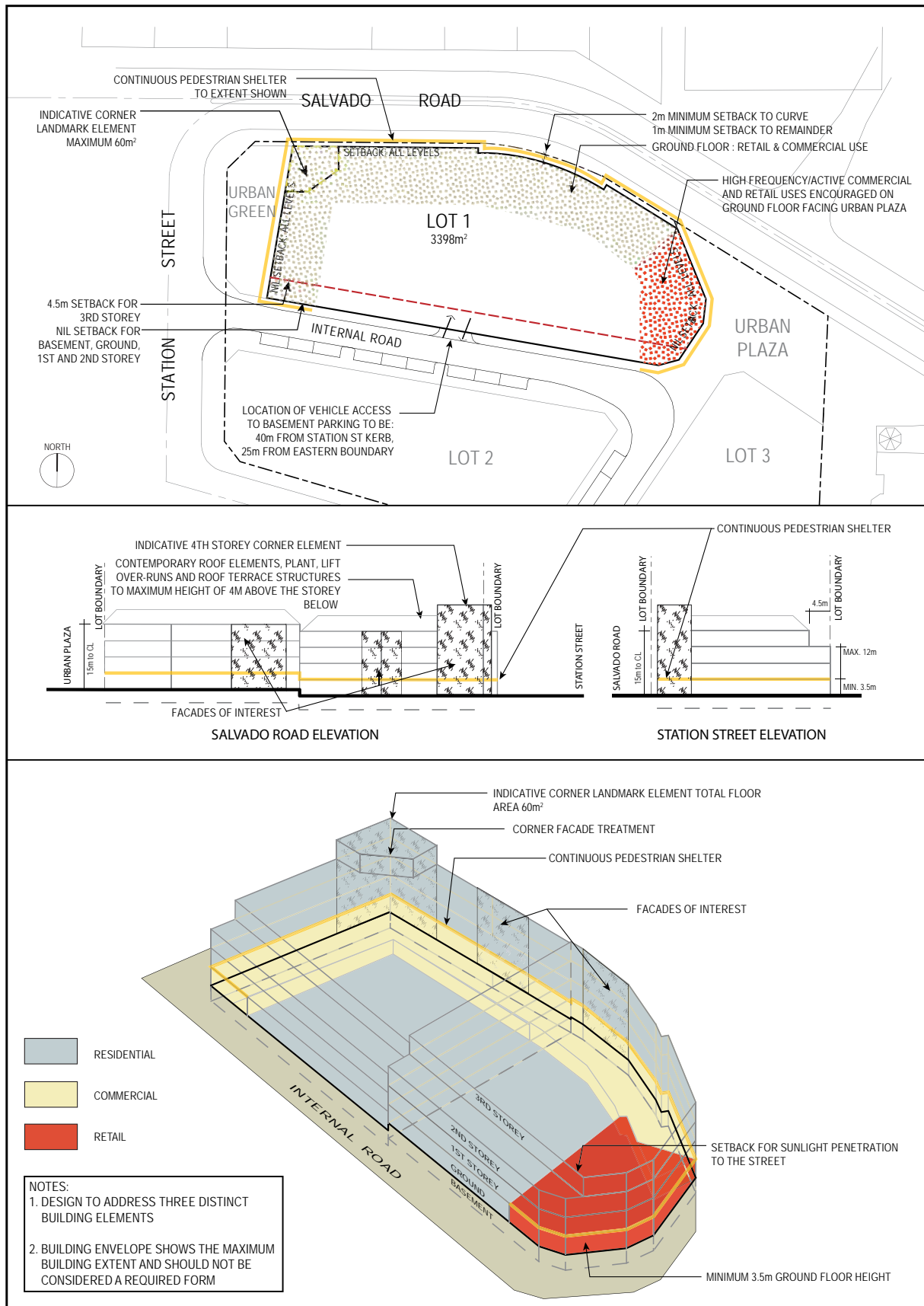
*The use should open onto the Urban Green, with entry doors and windows.*

All levels on the western side of Lot 1 are to address the Urban Green through the use of windows to active habitable rooms and useable balconies.

The built form should be built with a nil setback.

The floor level of the building adjacent to the Urban Green shall be such that at grade access can be equitably achieved.





# LOT SPECIFIC PROVISIONS

## LOT 2

### character statement

Lot 2 should incorporate a significant and identifiable building on the corner of Station Street and Wexford Street to act as a southern gateway to the precinct. Lot 2 will predominantly be residential in nature, with apartment style dwellings, however there will be some opportunities for compatible retail and commercial uses fronting Station Street.

The height and scale of development on this lot should reflect the height and scale of the buildings on the southern side of Wexford Street, while minimising overshadowing of those properties.

These provisions apply to all development on Lot 2, as indicated on the relevant Lot Diagram. Where there is an apparent discrepancy between these provisions and the general provisions, the lot specific provisions will apply.



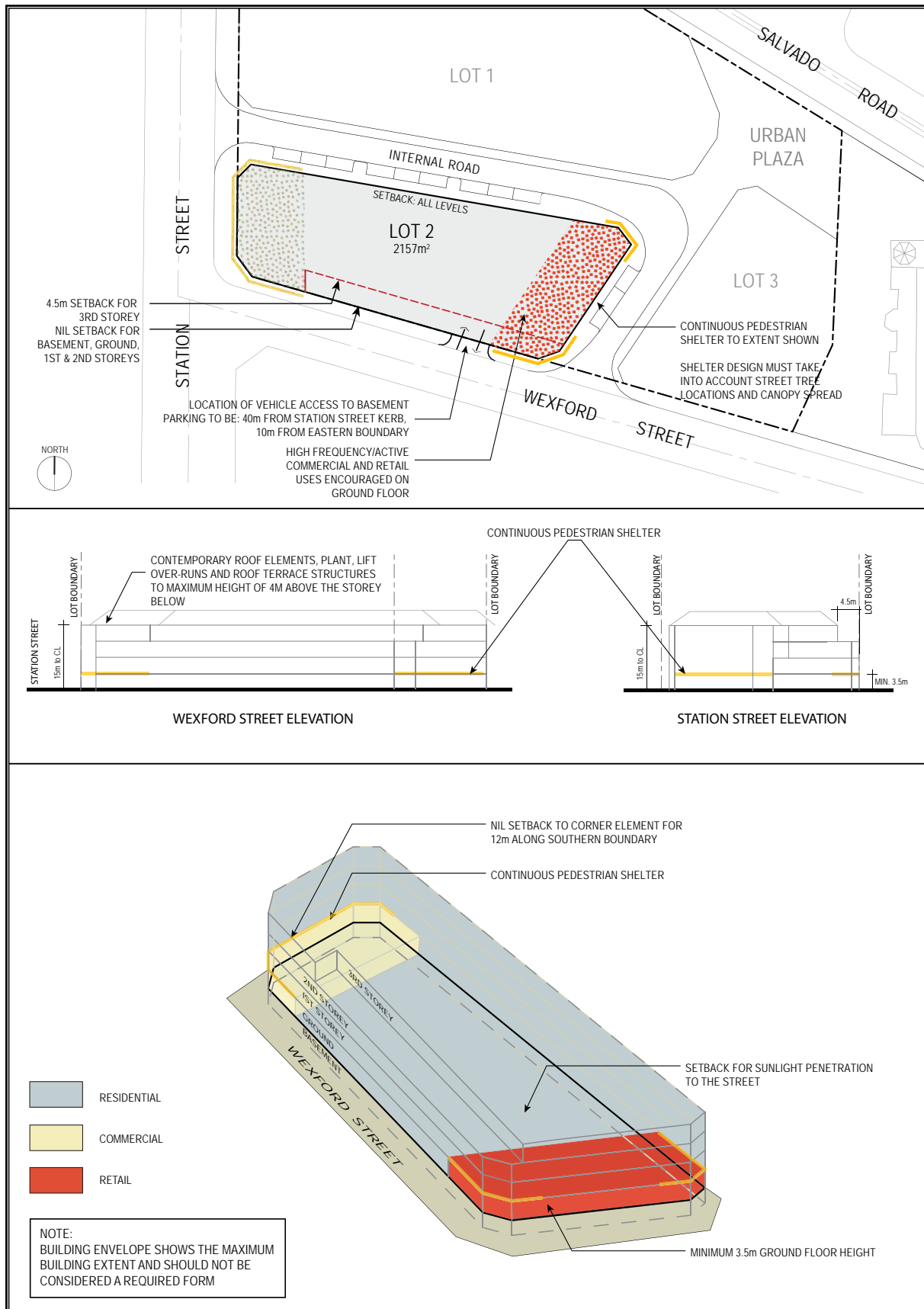
### land use

Land uses for Lot 2 are to be in accordance with the Scheme. Uses on the ground floor of Lot 2 shall include retail and commercial uses with activated frontages facing the north-south axis of the internal road such as Shops. The ground floor facing the east-west axis of the internal road may contain retail and commercial uses but is envisaged to contain mostly residential uses whilst the upper floors should also contain primarily residential uses.



### building articulation and roof form

Development on the south-western corner of Lot 2 is to be designed and articulated with a strong visual corner element that reads as a gateway to the precinct.



# LOT SPECIFIC PROVISIONS

## LOT 3

### character statement

Development on Lot 3 should consist of a building that overlooks and opens out on to the Urban Plaza. It is envisaged that the ground floor where the lot abuts the Urban Plaza may accommodate a café or restaurant with outdoor tables and chairs for alfresco dining. The upper floor that abuts the Urban Plaza should include generous balconies which encourage social activity and passive surveillance.

Development on Lot 3 should respect and recognise the significance of the adjoining St Josephs Church in terms of building bulk and scale. Development should provide an appropriate interface between the two sites that achieves noise attenuation whilst still providing passive surveillance of the Church car park. Development must not present a 'back of house' or large areas of blank walls to St Joseph's Church.

These provisions apply to all development on Lot 3, as indicated on the relative Lot Diagram. Where there is an apparent discrepancy between these provisions and the general provisions, the lot specific provisions will apply.



### land use

Land uses for Lot 3 are to be in accordance with the Scheme. Ground floor abutting the internal road and Urban Plaza shall contain activated retail and commercial uses such as Restaurants and Shops whilst the upper floors will contain residential uses.

### building height

Development of Lot 3 is to be generally 3 storeys but must be reduced to 2 storeys next to the Urban Plaza, as per the Lot Diagram, to retain a significant view corridor of the front portion of the church, including the church's spire, when viewed from Station Street through the precinct.





#### building articulation and roof form

A flat roof, concealed behind building parapets is required for development on Lot 3 where shown on the attached relevant Lot Diagram to retain a view corridor of the church spire on the adjacent lot. Alternatively, a roof terrace/balcony may be developed in this location provided that no structures (permanent or non-permanent) are erected such as pergolas or shade-sails.

#### relationship to urban plaza

The ground floor use of Lot 3 where it abuts the Urban Plaza shall be an active non-residential use, which is to address and open onto the Urban Plaza with window glazing and entry.

The floor level of the building adjacent to the Urban Plaza shall be such that at grade access can be equitably achieved.

#### levels

Lot 3 slopes from the Urban Plaza to Wexford Street. In addition to providing at-grade access to the Urban Plaza, development on Lot 3 shall also minimise grade separation at Wexford Street.

#### bins and service enclosures

Bins and service enclosures are to be designed to be an integral part of the building and screened from the street, urban plaza and adjacent St Joseph's Church. The eastern landscape area is not to be used for the storage of bins and other services.

# LOT SPECIFIC PROVISIONS

## LOT 3

### relationship to st josephs church

Development on Lot 3 adjacent to St Joseph's Church should be of an appropriate scale and bulk to respect the heritage status of the place and should not detract from the visual amenity of the church. The design of buildings on Lot 3 should not mock or mimic the design of the church.

Use of materials that complement the church construction, such as face brick, is appropriate for limited building elements to reference the Church's heritage significance.

Good design should be able to achieve "neighbourly" development without unduly compromising development potential.

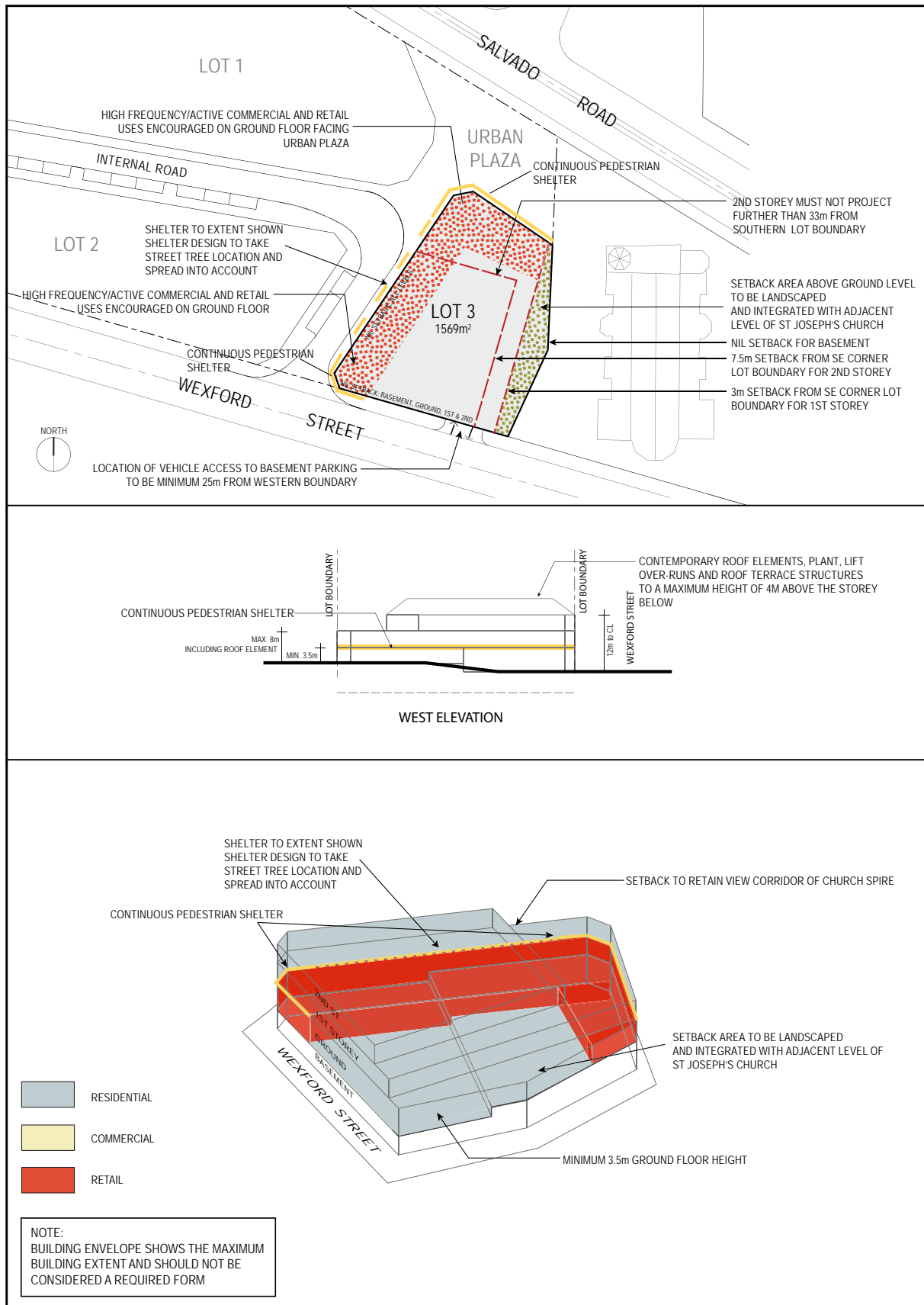
The eastern elevation facing St Joseph's Church is to provide depth and articulation to provide an outlook towards the Church.

Where living areas are located facing the eastern boundary, suitable acoustic measures are to be incorporated to the satisfaction of the Authority to minimise noise transfer between Lot 3 and the Church.

The eastern setback area adjacent to St Joseph's Church, is to be appropriately landscaped to assist as a buffer between the building and the Church.

Development must appropriately integrate with the existing retaining to St Joseph's Church at Wexford Street and the Urban Plaza to avoid creating gaps and entrapment areas in the urban form.







Further information on any aspect of these guidelines can be obtained by contacting the Authority's Planning Manager on (08) 9222 8000.

SUBIACO REDEVELOPMENT AUTHORITY

Locked Bag 8

Perth Business Centre

Perth WA 6849

Telephone (08) 9222 8000

Facsimilie (08) 9281 6020

[sra@sra.wa.gov.au](mailto:sra@sra.wa.gov.au)

