

# **CITY OF SUBIACO**

## **LOCAL PLANNING POLICY 1.1**

### **Residential Development – Single Houses and Grouped Dwellings**

#### **ADMINISTRATIVE**

| <b>Version</b> | <b>Date</b>      | <b>Comment</b>               |
|----------------|------------------|------------------------------|
| 0.1            | 07 July 2020     | Draft for public advertising |
| 1.0            | 23 February 2021 | Final for publishing         |

#### **AUTHORITY**

- Planning and Development (Local Planning Schemes) Regulations 2015.
- City of Subiaco Local Planning Scheme No. 5 (**Scheme**).
- State Planning Policy 7.3 – Residential Design Codes Volume 1 (**R-Codes Volume 1**).

#### **STATUTORY BACKGROUND**

This local planning policy (**the Policy**) is made pursuant to Schedule 2, Part 2, Division 2 of the *Planning and Development (Local Planning Schemes) Regulations 2015* relating to local planning policies.

This Policy applies in conjunction with the Scheme, the R-Codes Volume 1 and any other relevant local planning policies.

#### **PURPOSE**

The Policy provides a comprehensive basis for the control of single house and grouped dwelling development to ensure the existing residential character is protected, enhanced and allowed to evolve over time.

The Policy has a performance based approach to achieve high quality built form outcomes, while provide the necessary certainty to ensure the scale of development is consistent with the existing unique and valued character of neighbourhoods in the City of Subiaco.

#### **APPLICATION**

This Policy applies to land zoned Residential in the Scheme where residential development is proposed to which the R-Codes Volume 1 applies.

## **RELATIONSHIP WITH R-CODES VOLUME 1**

The R-Codes Volume 1 allows for unique development controls to be tailored to respond to local characteristics as follows:

- Clause 7.3.1(a) provides for the Policy to amend or replace specific deemed-to-comply provisions.
- Clause 7.3.1(b) provides for the Policy to augment the R-Codes by providing local housing objectives to guide judgement about the merits of any aspect of residential development that does not meet the requirements or is not provided for under the R-Codes.

This Policy:

1. Augments the R-Codes Volume 1 by providing local objectives for housing design and development that guide the consideration of the decision-maker to judge proposals; and
2. Amends or replaces deemed-to-comply provisions of the R-Codes Volume 1 as set out in Parts 3 to 6 of this Policy. In these instances as set out in Parts 3 to 6 of this Policy, the amended or replaced provisions prevail over the standard provision of the R-Codes Volume 1.

All other provisions of the R-Codes Volume 1 which are not amended or replaced continue to apply. The local housing objectives apply to aspects of residential development that is not provided for in the R-Codes. The relevant design principles of the R-Codes Volume 1 continue to apply in all circumstances.

## **POLICY**

### **1.0 Objectives**

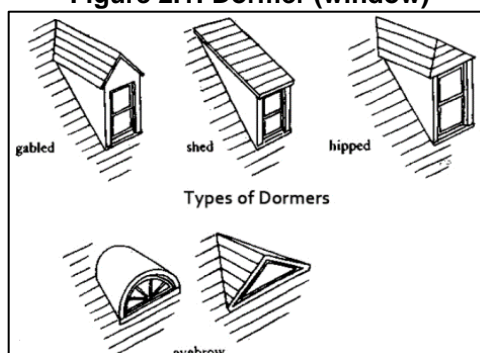
- (a) To protect and enhance the neighbourhood character and the amenity of the predominantly residential areas within the City of Subiaco.
- (b) To encourage any new development to reflect the existing character of the area through building orientation, bulk, scale, roof pitch and form, detailing and materials while encouraging high quality contemporary design that positively contributes to the existing streetscape.
- (c) In sub-precincts expected to change over time, to encourage new development to be of high quality contemporary design whilst being respectful to any nearby low density residential areas.
- (d) To enable design flexibility for single houses and grouped dwellings in sub-precincts transitioning to higher density and on land adjacent to higher density sub-precincts.
- (e) To encourage building design to activate the street, create a pedestrian-friendly environment and provide passive surveillance to the public realm.

## 2.0 Definitions

(a) The following terms are defined for the purpose of this Policy.

|                           |  |
|---------------------------|--|
| <b>Bulk:</b>              | Refers to the overall physical mass of a building as viewed from the street, public realm and from adjoining properties. The design elements that most notably contribute to bulk with respect to the streetscape are height, articulation, street setbacks and lot boundary setbacks. |
| <b>Character:</b>         | As defined in the City of Subiaco Local Planning Strategy 2020.  |
| <b>Deemed Provisions:</b> | Means the provisions contained in Schedule 2 of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i> .  |
| <b>Dormer (window):</b>   | An opening framed into a roof to provide natural lighting to the roof space, comprising a vertical window (i.e. a window with a greater vertical dimension than horizontal dimension) with its own generally pitched roof gable and side walls (Figure 2.1).                           |

Figure 2.1: Dormer (window)



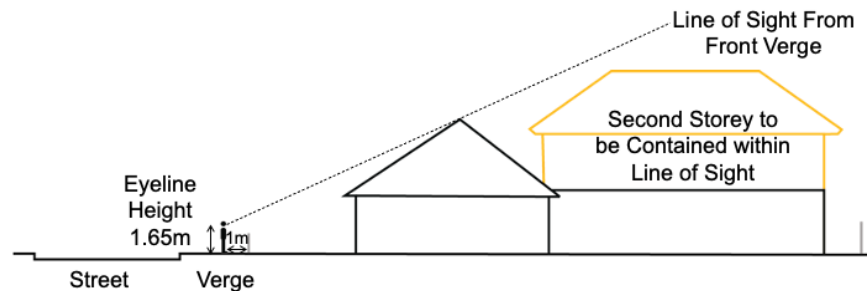
|   |  |
|---|--|
| <b>Grouped Dwelling:</b>                    | Means Grouped Dwelling development as defined in R-Codes Volume 1.   |
| <b>Established Neighbourhood Character:</b> | Means the dominant visual characteristics of the surrounding area formed by the interrelationship of built form, vegetation and formed topographic characteristics in both the private and public domains within that area. The elements of the built form that contribute to established neighbourhood character are orientation of buildings, window and door proportions, side setbacks, roof form and pitch, and bulk. These elements do not include colours and finishes. |
| <b>Established Street Setback Pattern:</b>  | Means street setbacks on the same side of the street within the surrounding area are consistent and do not vary by more than 1.0 metre.  |
| <b>Identified Heritage Significance:</b>    | Means those properties: <ul style="list-style-type: none"><li>• Entered in the Register of Heritage Places under the Heritage Act 2018; or</li><li>• included on a heritage list prepared in accordance with</li></ul>   |

- this Scheme; or
- Identified as contributory places located within an area designated under the Scheme as a heritage area.

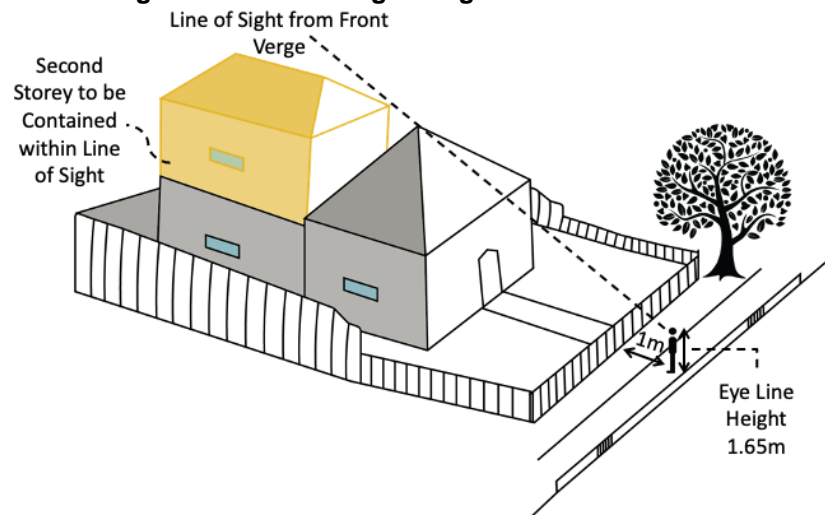
**Line of Sight:**

Line of sight is calculated 1 metre from outside the property boundary with an 'eye line' height of 1.65 metres (Figures 2.2 and 2.3).

**Figure 2.2: Line of sight diagram section view**



**Figure 2.3: Line of sight diagram 3D view**



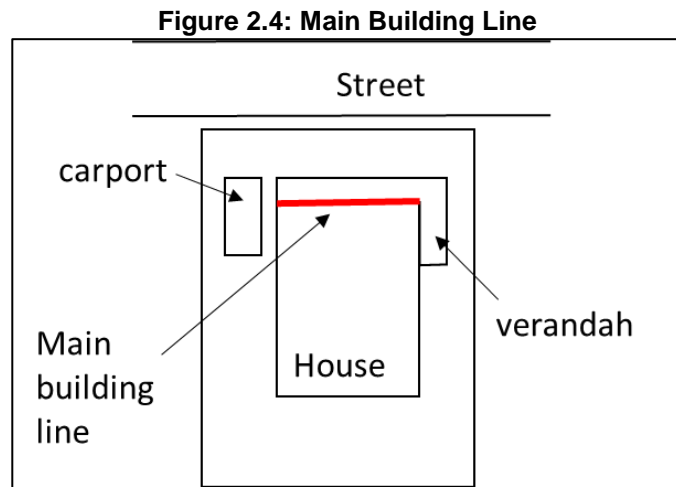
**Notes:**

- The line of sight for each property will vary depending on the wall height, roof pitch and overall height of the dwelling.
- Generally the line of sight is taken from the middle of the street boundary, but this may vary depending on the specific development – e.g. where the ground floor building line varies, the line of sight is taken from the 'worst case scenario'.
- The line of sight may not always mean that the upper floor is wholly concealed from view from the street – e.g. depending on the size of the upper floor, roof form and pitch, it may be visible either side of the ground floor ridge. However, it may still meet the line of sight requirement.
- Line of sight is measured from ridgeline of house, not from a verandah, carport, garage etc.

**Main Building Line:**

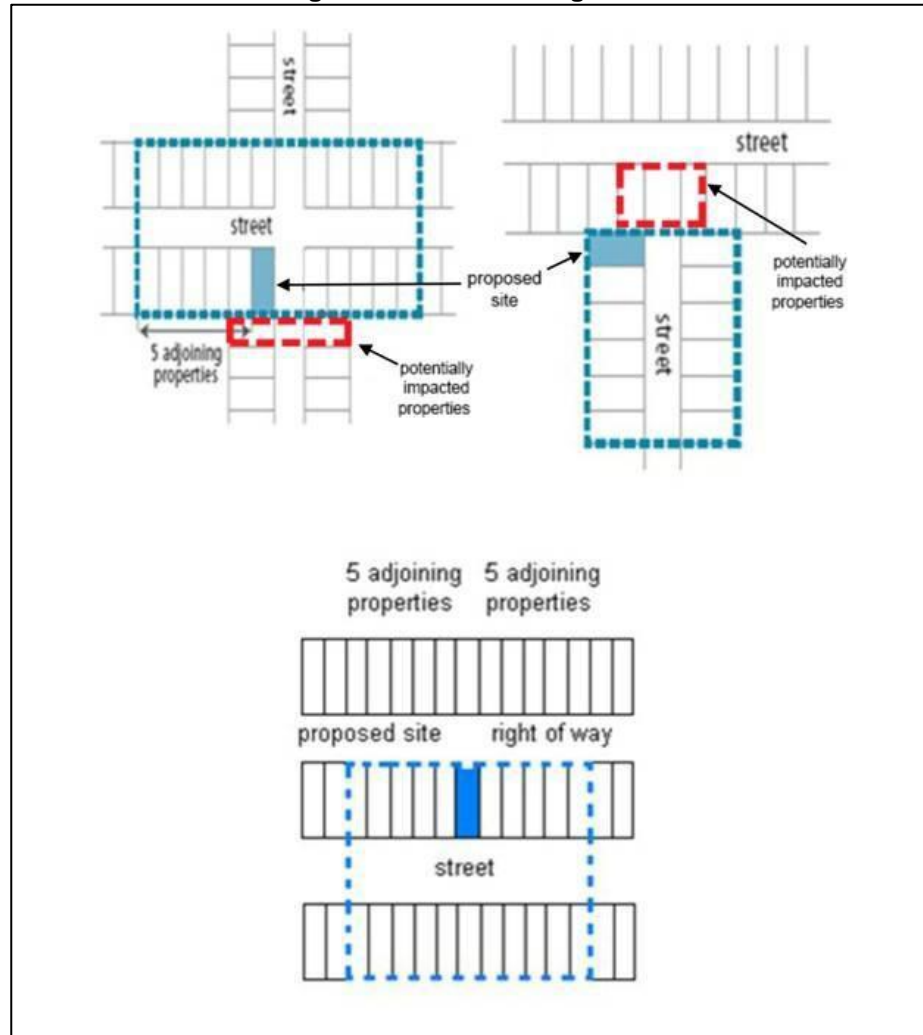
The point at which the wall of the main building is closest to the primary street frontage, but does not include walls of minor

projections such as verandahs, porticos, porches, bay windows and the like; or carports and garages (Figure 2.4).



|                          |  |
|--------------------------|--|
| <b>Precinct:</b>         | The City's residential precincts as shown on Map 1 in Part 8 of this Policy.   |
| <b>Predominant:</b>      | Where there is, in the surrounding area, an existing pattern of a design element or feature, nominally greater than 50%.   |
| <b>Scale:</b>            | The height, width and length of a building, or buildings, proposed within a development, relative to the built form of surrounding buildings.  |
| <b>Scheme:</b>           | Means the applicable Local Planning Scheme to which this Policy applies, being Local Planning Scheme No. 5 (LPS 5).  |
| <b>Single House:</b>     | Means Single House development as defined in R-Codes Volume 1.   |
| <b>Streetscape:</b>      | As defined in the Residential Design Codes Volume 2 – Apartments   |
| <b>Sub-precinct:</b>     | Land zoned Residential in the Scheme has been divided into four sub-precincts. Sub-precincts are shown on Map 8.2 in Part 8 of this Policy.  |
| <b>Surrounding Area:</b> | Means the five properties on either side of the proposed development on both sides of the street (excluding laneways/rights of way) that the house is orientated towards. In instances where there are less than 5 properties on either side of the proposed development, the surrounding area will be determined on a case by case basis having due regard to the total number of properties on either side of the proposed development on both sides of the street, at the discretion of the City of Subiaco (Figure 2.5). |

**Figure 2.5: Surrounding Area**



**Transition Area:**

An area which has been increased in density under LPS 5 and is expected to undergo a period of transition from predominantly single houses to grouped and multiple dwellings. These areas are generally captured within Sub-Precinct 4 areas and shown on Map 8.3 in Part 8 of this Policy.

- (b) All other terms, words and expression used in this policy have the same meaning as they have in the Scheme and the applicable planning framework.

### **3.0 General Provisions for Sub-Precincts 1 and 2**

3.1 The general provisions in Part 3 of this Policy apply to Sub-Precincts 1 and 2 unless a sub-precinct 2 provision in Table 2 conflicts, in which case Table 2 prevails over Table 1 to the extent of any conflict.

3.2 Table 1 sets out the general development requirements for Sub-Precincts 1 and 2.

#### **3.3 Local Housing Objectives**

The following local housing objectives guide decision-making on matters not provided for under the R-Codes Volume 1.

##### **3.3.1 Roof Form and Pitch**

- (a) To ensure the proposed roof form and pitch as viewed from the street complements the roof form and pitch evident in the surrounding area.
- (b) To ensure the proposed roof form and pitch of any additions where visible from the street complements the roof form and pitch of the existing house.

##### **3.3.2 Design of Carports and Garages**

- (a) To ensure the proposed development is consistent with the established neighbourhood character.
- (b) To minimise the impact of the proposed carport or garage on the dwelling and the streetscape, particularly with respect to retaining the visual dominance of the dwelling.

##### **3.3.3 Interface and Transition Areas**

- (a) To ensure areas transitioning to different density codes are considered appropriately within both their existing and desired future context.
- (b) To ensure areas at the interface of different densities are considered appropriately within both their existing and desired future context.

##### **3.3.4 Minimum Frontage**

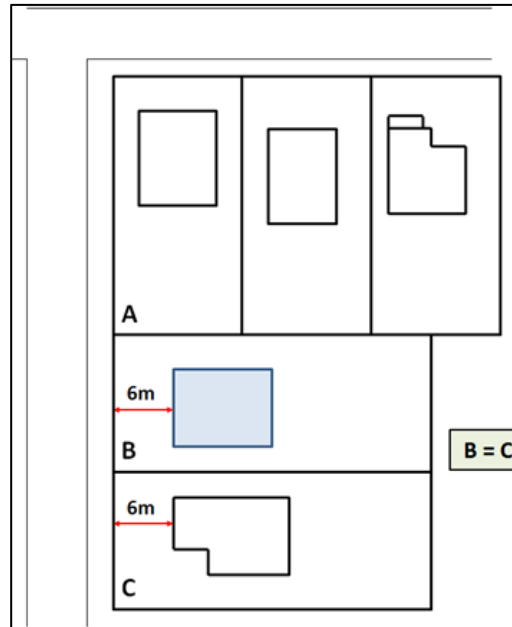
- (a) In areas coded R15 and R20, to enable subdivision to occur at a similar pattern to the existing lot pattern in the surrounding area, a minimum lot frontage of 8.5 metres may be appropriate where lot frontages of less than that stipulated in Table 1 of the R-Codes are evident.

**Table 1 – General Provisions for Sub-Precincts 1 and 2**

| Design Principle            | Deemed-to-Comply Provisions   |
|-----------------------------|---|
| As per R-Codes Volume 1     | Amended or replaced R-Codes Volume 1 provision  |
| <b>5.1.2 Street Setback</b> |   |
| P2.1                        | <p>Design Elements 5.1.2 C2.1 and C2.2 of the R-Codes are replaced with the following:</p> <p>C2.1 The main building line setback from the primary street boundary:</p> <ul style="list-style-type: none"> <li>i. The ground floor of buildings setback from the primary street: <ul style="list-style-type: none"> <li>a) Corresponding to the average of the setback of the two nearest dwellings on the same side of the road facing the same street, measured to the main building line.</li> </ul> </li> </ul> <div style="display: flex; justify-content: space-around; align-items: flex-end;"> <div data-bbox="638 730 1146 1190"> <p>Diagram illustrating the calculation of the average setback for three separate dwellings (A, B, and C) with setbacks of 4m, 5m, and 6m. The formula shown is <math>B = \frac{A + C}{2}</math>.</p> </div> <div data-bbox="1258 719 1852 1190"> <p>Diagram illustrating the calculation of the average setback for two dwellings (A and B) with setbacks of 4m and 5m, and a third dwelling (C) with a setback of 6m. The formula shown is <math>B = \frac{A + C}{2}</math>.</p> </div> </div> |



- b) Where at the end of the street, equal to the nearest dwelling on the same side of the road facing the street.



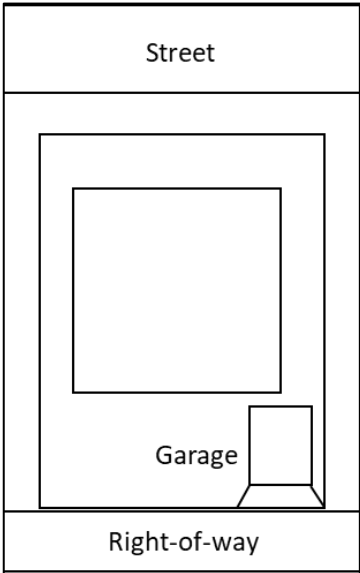
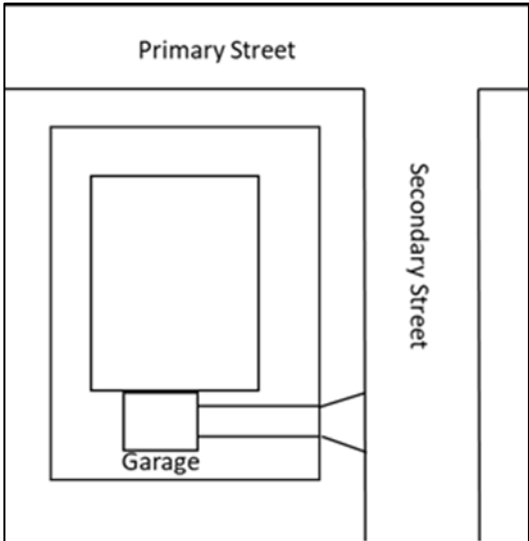
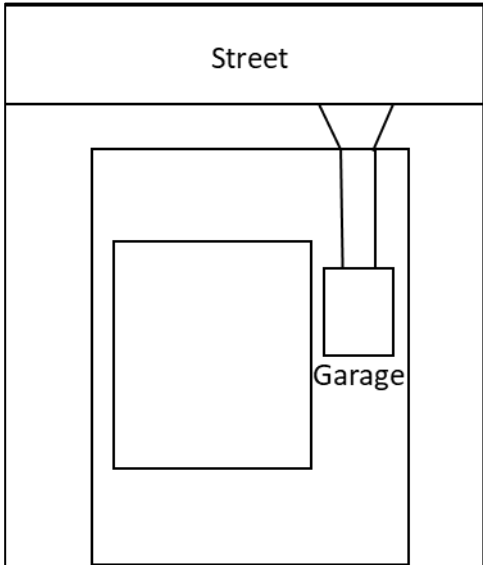
- c) An unenclosed porch, balcony, verandah or the equivalent may (subject to the Building Code of Australia) project into the primary street setback area to a maximum of 2 metres.
- ii. Where considered appropriate under 5.1.6 Building height P6 of the R-Codes, the upper floors of buildings setback from the primary street:
- a) So as to be below the line of sight in order to maintain the appearance of a single storey streetscape when viewed from the primary street; or
  - b) So as to be wholly contained within the roof space.

|      |   |
|------|---|
|      | <p>C2.2 Buildings setback from the secondary street boundary:</p> <ul style="list-style-type: none"> <li>i. In accordance with Table 1 of the R-Codes.</li> <li>ii. Where applicable, the upper floor of all development is to be setback a minimum of 1.5 metres from the ground floor secondary street facade.</li> </ul> <p><i>C2.3 Buildings set back from the corner truncation in accordance with the secondary street setback in Table 1.</i></p>  |
| P2.2 | <p>Design Elements 5.1.2 C2.4 of the R-Codes is replaced with the following:</p> <p>C2.4 Buildings mass and form to comply with the following:</p> <ul style="list-style-type: none"> <li>i. For new development, other than additions, the following applies: <ul style="list-style-type: none"> <li>a) Roof pitch visible from the street to reflect the predominant roof pitch of the surrounding area as it relates to the street.</li> <li>b) Where no predominant roof pitch is evident in the surrounding area, the roof pitch to be within the range of pitches evident in the surrounding area.</li> <li>c) Roof form to complement the surrounding area. A contemporary interpretation is acceptable.</li> </ul> </li> <li>ii. Additions to reflect the roof form and pitch of the existing dwelling where visible from the street.</li> <li>iii. Use of a combination of contemporary and/or traditional materials to complement the surrounding area. In the case of additions, materials are to complement the existing dwelling.</li> <li>iv. Facades to be articulated to provide interest to the street. Examples include the use of verandahs, different materials and finishes, stepped facades and similar.</li> <li>v. No blank walls and featureless facades to face the street.</li> <li>vi. Design elements visible from the street to reflect the scale and proportion of traditional building elements where this is predominant in the surrounding area.</li> </ul> |

### 5.1.6 Building height

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|----|---|
| P6 | <p>C6.1 Buildings which comply with Table 3 of the R-Codes as modified by clause 26(1) of LPS 5 for category A area buildings.</p> <p>C6.2 Dormers are not included in the measurement of wall height except where the front wall of a dormer runs contiguous with a wall below it.</p> |
|----|---|

### 5.2.1 Setback of garages and carports

|                         |   |
|-------------------------|---|
| <p>P1.1</p> <p>P1.2</p> | <p>Design Elements 5.2.1 C1.1, C1.2, C1.4 and C1.5 of the R-Codes are replaced with the following:</p> <p>C1.1 Having regard to access and site constraints, garages and carports are to be located:</p> <ol style="list-style-type: none"> <li>At the rear of the property behind the dwelling, to be accessed either from a right-of-way where available ; or</li> <li>1m behind the main building line adjacent to the secondary street where the garage or carport cannot be located in accordance with C1.1.i; or</li> <li>1m behind the main building line adjacent to the primary street where no alternative location is available.</li> </ol> <div style="display: flex; justify-content: space-around; align-items: flex-end;"> <div data-bbox="398 687 757 1300" style="text-align: center;">  <p>At rear with access from ROW</p> </div> <div data-bbox="920 687 1447 1339" style="text-align: center;">  <p>Minimum 1m behind main building line with access from secondary street</p> </div> <div data-bbox="1529 687 2009 1339" style="text-align: center;">  <p>Minimum 1m behind main building line from the primary street</p> </div> </div> <p>C1.2 Where no other option exists to locate a carport or garage in accordance with 5.2.1 C1.1:</p> |
|-------------------------|---|

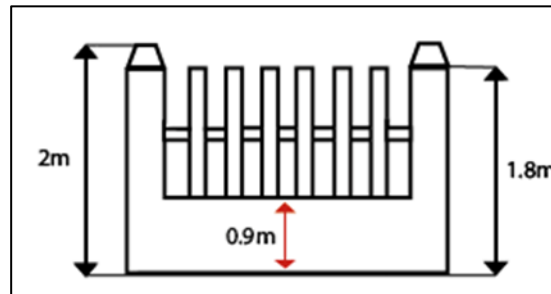
|                                      |   |
|--------------------------------------|---|
|                                      | <ul style="list-style-type: none"> <li>i. Garages are not acceptable in the front setback area; and</li> <li>ii. Single width carports within the primary or secondary setback area are permitted where they are a predominant feature within the surrounding area; and</li> <li>iii. the width of the carport shall not exceed 50% of the width of the dwelling façade, and the construction shall allow an unobstructed view between the dwelling and the street; and</li> <li>iv. Carports within the front setback area to be: <ul style="list-style-type: none"> <li>a) Designed to the minimum dimensions of the relevant Australian Standard; and</li> <li>b) Lightly framed and roofed, allowing views between the dwelling and the street.</li> </ul> </li> </ul>  |
|                                      | <i>C1.3 Garages and carports built up to the boundary abutting a communal street or right-of-way which is not the primary or secondary street boundary for the dwelling, with manoeuvring space of at least 6m, located immediately in front of the opening to the garage or carport and permanently available.</i>   |
| <b>5.2.2 Garage width</b>            |   |
| P2                                   | <p>Design element 5.2.2 C2 of the R-Codes is replaced with the following:</p> <p>C2 Where a garage is permitted under 5.2.1 C1.1 ii. and iii. a garage door and its supporting structures is not to occupy more than 50 per cent of the frontage at the main building line as viewed from the street.</p>   |
| <b>5.2.3 Street surveillance</b>     |   |
| P3                                   | <p>Design elements 5.2.3 C3.1 and 5.2.3 C3.2 of the R-Codes are replaced with the following:</p> <p>C3.1 The primary street facade of any new development or redevelopment of an existing dwelling is to be orientated towards the primary street ensuring that:</p> <ul style="list-style-type: none"> <li>i. the entrance to a dwelling is clearly visible and accessible from the primary street; and</li> <li>ii. at least one major opening to a habitable room has a clear view of the primary street.</li> </ul> <p>C3.2 In the case of corner lots, the secondary street facade is to be orientated towards the secondary street ensuring that:</p> <ul style="list-style-type: none"> <li>i. the secondary street facade is articulated with windows; and</li> <li>ii. at least one major opening to a habitable room has a clear view of the secondary street.</li> </ul> |
|                                      | <i>C3.3 For battleaxe lots or sites with internal driveway access, at least one major opening from a habitable room of the dwelling faces the approach to the dwelling.</i>   |
| <b>5.2.4 Street walls and fences</b> |   |

P4

Design element 5.2.4 C4.1 and C4.2 of the R-Codes are replaced with the following:

C4.1 Front fences within the primary street setback area to comply with the following:

- i. visually permeable above a height of 900mm above natural ground level, measured from the primary street side of the front fence, to allow for passive surveillance and visual interaction between the dwelling and the street; and
- ii. reflect the height and style of fences predominant within the surrounding area, to a maximum overall height of 1.8 metres for panels, and maximum 2 metres for the piers above natural ground level, measured from the primary street side of the front fence; and
- iii. materials and design of the front fence to complement the dwelling and to be sympathetic to the surrounding area.



C4.2 Street walls and fences on land fronting either Selby Street, Hay Street, Thomas Street, Railway Road or Aberdare Road are to comply with the following:

- i. Solid fencing to a maximum height of 1.8 metres above natural ground level, measured from the primary street side of the front fence; and
- ii. The maximum height of piers with decorative capping to be 2 metres above natural ground level, measured from the primary street side of the front fence.

C4.3 Street walls and fences that abut a secondary street and which screen an outdoor living area behind the primary street setback line are to meet the relevant standards for a front fence contained in 5.2.4 C4.2 above.

C4.4 Street walls and fences that abut a secondary street and which do not screen an outdoor living area are to meet the relevant standards for a front fence contained in 5.2.4 C4.1 above.

|  |   |
|--|---|
|  | <p>C4.5 Gatehouses and porticos are not permitted within the primary street setback area unless they are evident within the surrounding area. Where gatehouses and porticos are considered acceptable as they are evident in the surrounding area, the following applies:</p> <ul style="list-style-type: none"> <li>i. To be integrated in the design of the front fence; and</li> <li>ii. Built to a maximum width of no more than 20% of the width of the lot; and</li> <li>iii. No more than 2.4 metres in wall height and 3.5 metres in overall height; and</li> <li>iv. Lighting on the exterior of the portico or gatehouse is to be provided; and</li> <li>v. The gate is to have a visually permeable to allow clear views between the dwelling and the street; and</li> <li>vi. The portico or gatehouse is to reflect the style of the front fence.</li> </ul> |
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Note: provisions in *italics* are unchanged from the R-Codes Volume 1

#### **4.0 Sub-Precinct 2**

- 4.1 Part 3 General Provisions apply to Sub-Precinct 2, unless a sub-precinct provision in Table 2 conflicts, in which case Table 2 prevails over Table 1 to the extent of any conflict.
- 4.2 Table 2 sets out the specific development requirements for Sub-Precinct 2.

**Table 2 – Sub-Precinct 2 Provisions**

| Design Principle                             | Deemed-to-Comply Provisions   |
|--|---|
| As per R-Codes Volume 1                      | Amended or replaced R-Codes Volume 1 provision  |
| <b>5.2.1 Setback of garages and carports</b> |   |
| P1.1<br>P1.2                                 | <p>Design Elements 5.2.1 C1.1, C1.2, C1.4 and C1.5 of the R-Codes are replaced with the following:</p> <p>C1.1 Having regard to access and site constraints, garages are to be located:</p> <ol style="list-style-type: none"> <li>At the rear of the property behind the dwelling, to be accessed either from a right-of-way where available or an internal single width driveway; or</li> <li>1m behind the main building line adjacent to the secondary street where the garage cannot be located in accordance with C1.1.i; or</li> <li>1m behind the main building line adjacent to the primary street where no alternative location is available.</li> </ol> <div style="display: flex; justify-content: space-around; align-items: flex-end;"> <div data-bbox="405 719 743 1262"> <p>At rear with access from ROW</p> </div> <div data-bbox="936 719 1397 1198"> <p>Minimum 1m behind main building line with access from secondary street</p> </div> <div data-bbox="1599 719 2018 1286"> <p>Minimum 1m behind main building line of the primary street</p> </div> </div> |



|                                      |   |
|--------------------------------------|---|
|                                      | <p>C1.2 Carports to be located in accordance with 5.2.1 C1.1 above; or:</p> <ul style="list-style-type: none"> <li>i. Carports within the primary or secondary setback area to be: <ul style="list-style-type: none"> <li>a) Designed to the minimum dimensions of the relevant Australian Standard; and</li> <li>b) Lightly framed and roofed, allowing views between the dwelling and the street.</li> </ul> </li> </ul>  |
|                                      | <p><i>C1.3 Garages and carports built up to the boundary abutting a communal street or right-of-way which is not the primary or secondary street boundary for the dwelling, with manoeuvring space of at least 6m, located immediately in front of the opening to the garage or carport and permanently available.</i></p>  |
| <b>5.2.4 Street walls and fences</b> |   |
| P4                                   | <p>Design element 5.2.4 C4.1 and C4.2 of the R-Codes are replaced with the following:</p> <p>C4.1 Front fences to comply with the following:</p> <ul style="list-style-type: none"> <li>i. A maximum height of 0.9 metres above natural ground level, measured from the primary street side of the front fence; and</li> <li>ii. The maximum height of piers with decorative capping to be 1.0 metre above natural ground level, measured from the primary street side of the fence; and</li> <li>iii. materials and design of the front fence to complement the dwelling and to be sympathetic to the surrounding area; and</li> <li>iv. gatehouses and porticos are not permitted.</li> </ul> <p>C4.2 Street walls and fences that abut a secondary street are to comply with the following:</p> <ul style="list-style-type: none"> <li>i. Solid fencing to a maximum height of 1.8 metres above natural ground level, measured from the primary street side of the front fence; and</li> <li>ii. The maximum height of piers with decorative capping to be 2 metres above natural ground level, measured from the primary street side of the front fence.</li> </ul> |

Note: provisions in *italics* are unchanged from the R-Codes Volume 1

## 5.0 Sub-precinct 3

5.1 Development is to be in accordance with the provisions of the Scheme, R-Codes and relevant policies unless otherwise specified in Part 5 of this policy.

5.2 Table 3 sets out the building height requirements for Sub-Precinct 3.

### 5.3 Local Housing Objectives

The following local housing objectives guide decision-making on matters not provided for under the R-Codes:

#### 5.3.1 Minimum Frontage

- (a) In areas coded R15 and R20, to enable subdivision to occur at a similar pattern to the existing lot pattern in the surrounding area, a minimum lot frontage of 8.5 metres may be appropriate where lot frontages of less than that stipulated in Table 1 of the R-Codes are evident.

**Table 3 – Sub-Precinct 3 Provisions**

| Design Principle             | Deemed-to-Comply Provisions  |
|------------------------------|--|
| As per R-Codes Volume 1      | Amended or replaced R-Codes Volume 1 provision   |
| <b>5.1.6 Building height</b> |  |
| P6                           | <p>C6.1 Buildings which comply with Table 3 of the R-Codes for category B area buildings.</p> <p>C6.2 On land with a density code of R50 and above, buildings to comply with Table 3 of the R-Codes for category C area buildings where they meet the following:</p> <ul style="list-style-type: none"><li>i. Do not abut on any lot boundary land with a density code of R30 or lower; and</li><li>ii. Are designed to minimise any negative impact on the streetscape and any adjoining properties, including:<ul style="list-style-type: none"><li>a) overshadowing of any active habitable spaces, or outdoor living areas of adjoining properties; and</li><li>b) visual privacy impacts on active habitable spaces or outdoor living areas of adjoining properties; and</li><li>c) the appearance of bulk and blank walls facing adjoining properties.</li></ul></li></ul> |

#### 5.2.4 Street walls and fences

|    |   |
|----|---|
| P4 | <p>Design element 5.2.4 C4.1 and C4.2 of the R-Codes are replaced with the following:</p> <p>C4.1 Front fences within the primary street setback area that are visually permeable above a height of 1.2m above natural ground level, measured from the primary street side of the front fence, to allow for passive surveillance and visual interaction between the dwelling and the street and have a maximum overall height of 1.8 metres for panels, and maximum 2 metres for the piers above natural ground level, measured from the primary street side of the front fence.</p> <p>C4.2 Street walls and fences on land fronting either Selby Street, Hay Street, Thomas Street, Railway Road or Aberdare Road are to comply with the following:</p> <ul style="list-style-type: none"><li>i. Solid fencing to a maximum height of 1.8 metres above natural ground level, measured from the primary street side of the front fence; and</li><li>ii. The maximum height of piers with decorative capping to be 2 metres above natural ground level, measured from the primary street side of the front fence.</li></ul> |
|----|---|

Note: provisions in *italics* are unchanged from the R-Codes Volume 1

## **6.0 Sub-precinct 4**

6.1 Development is to be in accordance with the provisions of the Scheme, R-Codes and relevant policies, unless otherwise specified in Part 6 of this policy.

6.2 Table 4 sets out the specific development requirements for Sub-Precinct 4.

6.3 Local Housing Objectives for Sub-Precinct 4:

The following local housing objectives guide decision-making on matters not provided for under the R-Codes

### **6.3.1 Street setbacks:**

- (a) Where no established streetscape exists as densities have been increased and the streetscape is expected to substantially change as a result, buildings should be set back to contribute to the future desired streetscape.
- (b) Land at the interface of different densities should provide a transition between the primary street setbacks of both densities.

### **6.3.2 Interface and transition areas**

- (a) To ensure areas transitioning to different density codes are considered appropriately within both their existing and desired future development context and streetscape.
- (b) To ensure areas at the interface of different densities are considered appropriately within both their existing and desired future development context and streetscape.

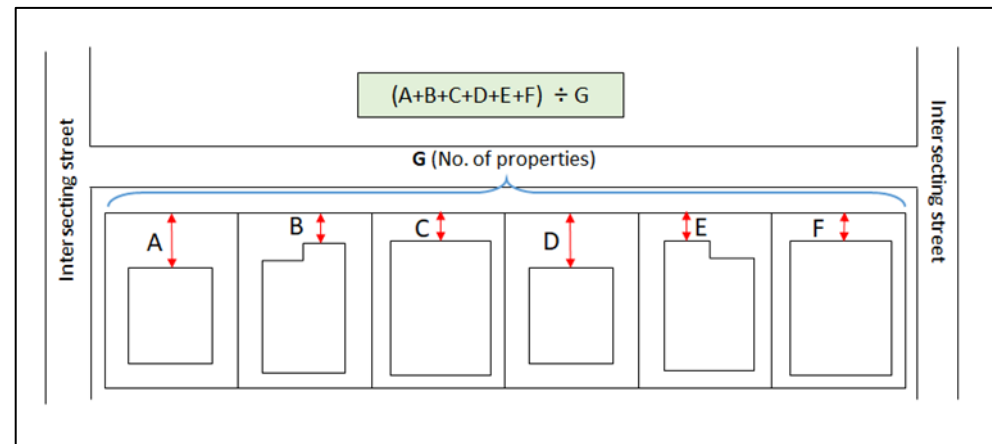
### **6.3.3 Minimum Frontage**

- (a) In areas coded R15 and R20, to enable subdivision to occur at a similar pattern to the existing lot pattern in the surrounding area, a minimum lot frontage of 8.5 metres may be appropriate where lot frontages of less than that stipulated in Table 1 of the R-Codes are evident

**Table 4 – Sub-Precinct 4 Provisions**

| Design Principle            | Deemed-to-Comply Provisions   |
|-----------------------------|---|
| As per R-Codes Volume 1     | Amended or replaced R-Codes Volume 1 provision  |
| <b>5.1.2 Street Setback</b> |   |
| P2.1                        | <p>Design Elements 5.1.2 C2.1 and C2.2 of the R-Codes are replaced with the following:</p> <p>C2.1 Buildings to be set back from the primary street boundary:</p> <p>i. Where a side boundary of the development site is located at the interface of two different densities, the setback is to be determined by the average of the minimum setback of both densities;</p> <div data-bbox="763 687 1722 1206" data-label="Diagram"> <p>The diagram illustrates the street setback calculation for a development site at the interface of two densities. The street is at the top. Below it, the development site is divided into three sections: A (left, density ≥R50), B (middle, density ≥R50), and C (right, density ≤R40). Section A has a setback of 2m. Section B has a setback of 3m. Section C has a setback of 4m. A green box to the right contains the formula <math>B = \frac{A + C}{2}</math>.</p> </div> |

- ii. Where an established street setback pattern exists, the primary street setback to reflect the established street setback; or



- iii. Only where the above provisions do not apply, in accordance with the provisions of the R-Codes.

**C2.2 Buildings setback from the secondary street or right-of-way boundary:**

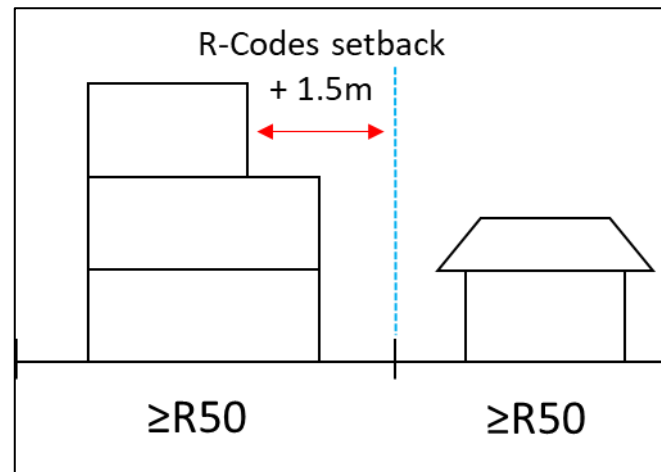
- i. Where the development site is located at the interface of two different densities across the secondary street or a right-of-way, the upper floor of all development is to be setback a minimum of 1.5 metres from the ground floor secondary street façade; or
- ii. Only where the above provision does not apply, in accordance with Table 1 of the R-Codes.

P2.2

Design Elements 5.1.2 C2.4 of the R-Codes is replaced with the following:

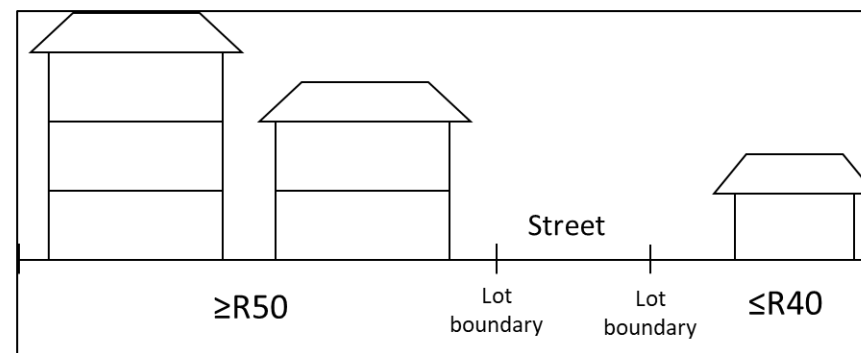
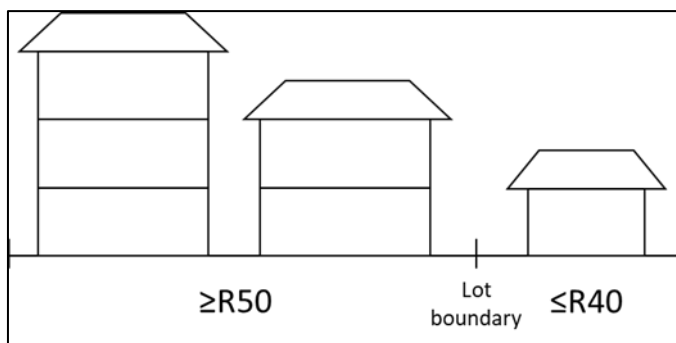
C2.4 Buildings mass and form to comply with the following:

- i. The design of new development to be high quality and contemporary in style whilst being respectful to the existing character of adjoining low density areas and to the bulk, scale and proportions of existing dwellings and streetscapes within areas transitioning to a higher density.
- ii. Facades to be articulated to provide interest to the street. Examples include the use of verandahs, different materials and finishes, stepped facades and similar.
- iii. No blank walls and featureless facades to face the street.
- iv. Where a development interfaces with lower density residential land, the development is to be designed to reduce any negative impacts on the lower density land with regard to matters such as overshadowing, visual privacy, building bulk.
- v. Notwithstanding the building height permitted in accordance with 5.1.6 C6.2, where a development abuts an existing single storey dwelling, any portion with a building height above the maximum wall height for Category B buildings is to be setback an additional 1.5 metres from the lot boundary adjacent to the single storey dwelling.



### 5.1.6 Building height

- P6
- C6.1 Buildings on land with a density code of R40 or less to comply with Table 3 of the R-Codes for category B area buildings.
- C6.2 Buildings on land with a density code of R50 and above to comply with Table 3 of the R-Codes for category C area buildings, subject to compliance with 5.1.2C2.4v.
- C6.3 On land abutting land with a density code of R40 or less (including across a public street), a single house, or a grouped dwelling abutting the lower density land, to comply with Table 3 of the R-Codes for category B area buildings.



### 5.2.3 Street surveillance

- P3
- 5.2.3C3.1 and 5.2.3C3.2 of the Codes are replaced with the following:
- C3.1 The primary street facade of any new development or redevelopment of an existing dwelling is to be orientated towards the primary street ensuring that:
- the entrance to a development is clearly visible and accessible from the primary street; and
  - at least one major opening to a habitable room has a clear view of the primary street.
- C3.2 In the case of corner lots, the secondary street facade is to be orientated towards the secondary street ensuring that:
- the secondary street facade is articulated with windows; and



|                                      |   |
|--------------------------------------|---|
|                                      | <p>ii. at least one major opening to a habitable room has a clear view of the secondary street.</p>   |
|                                      | <p><i>C3.3 For battleaxe lots or sites with internal driveway access, at least one major opening from a habitable room of the dwelling faces the approach to the dwelling.</i></p>  |
| <b>5.2.4 Street walls and fences</b> |   |
| P4                                   | <p>Design element 5.2.4 C4.1 and C4.2 of the R-Codes are replaced with the following:</p> <p>C4.1 Front fences to comply with the following:</p> <ul style="list-style-type: none"> <li>i. visually permeable above a height of 900mm from natural ground level, measured from the primary street side of the front fence, to allow for passive surveillance and visual interaction between the dwelling and the street; and</li> <li>ii. A maximum overall height of 1.8 metres above natural ground level, measured from the primary street side of the front fence; and</li> <li>iii. The maximum height of piers with decorative capping to be 2.0 metre above natural ground level, measured from the primary street side of the fence; and</li> <li>iv. materials and design of the front fence to complement the dwelling and to be sympathetic to the streetscape.</li> </ul> <p>C4.2 Street walls and fences that abut a secondary street are to comply with the following:</p> <ul style="list-style-type: none"> <li>i. Solid fencing to a maximum height of 1.8 metres above natural ground level, measured from the primary street side of the front fence; and</li> <li>ii. The maximum height of piers with decorative capping to be 2 metres above natural ground level, measured from the primary street side of the front fence.</li> </ul> |

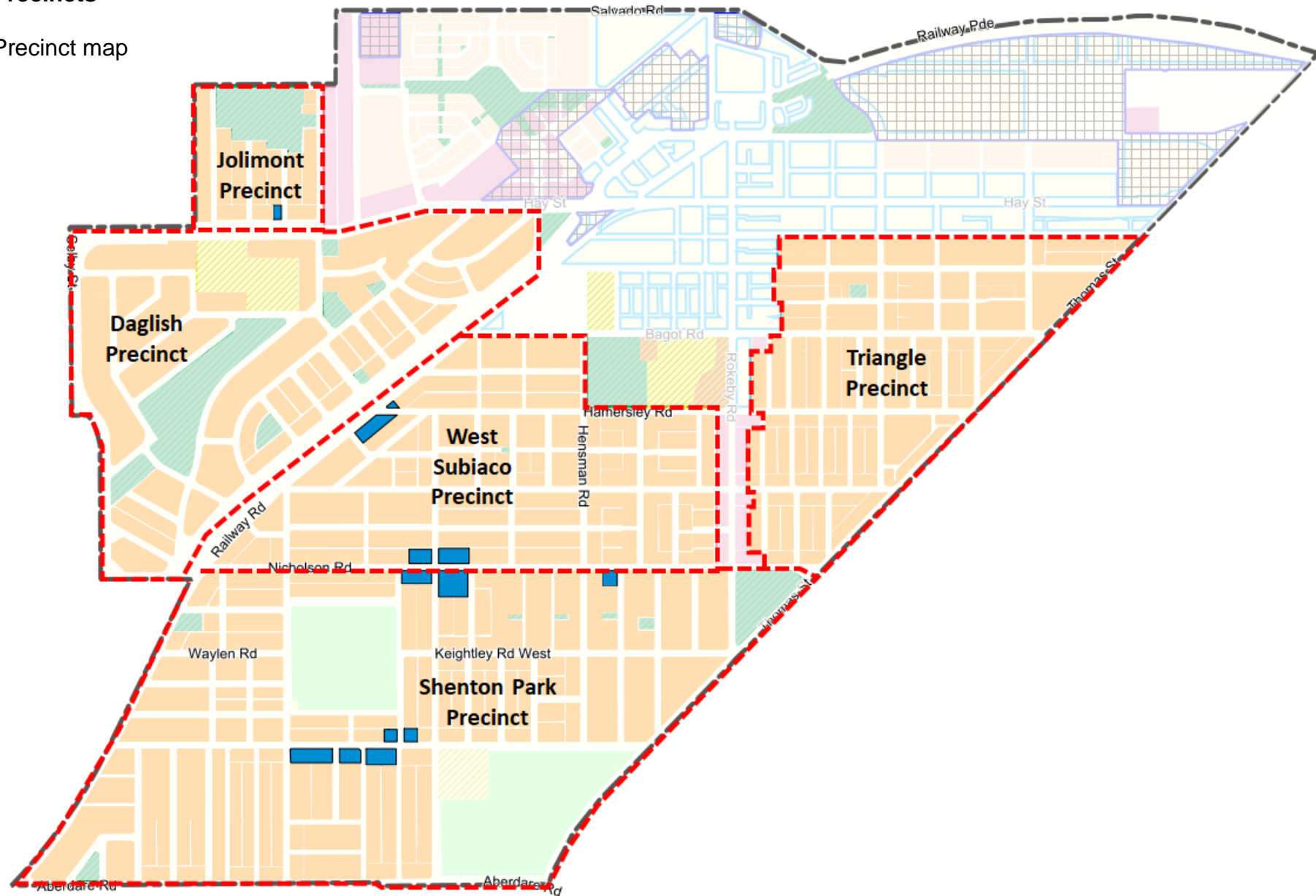
Note: provisions in *italics* are unchanged from the R-Codes Volume 1

## 7.0 Sub-Precinct 5

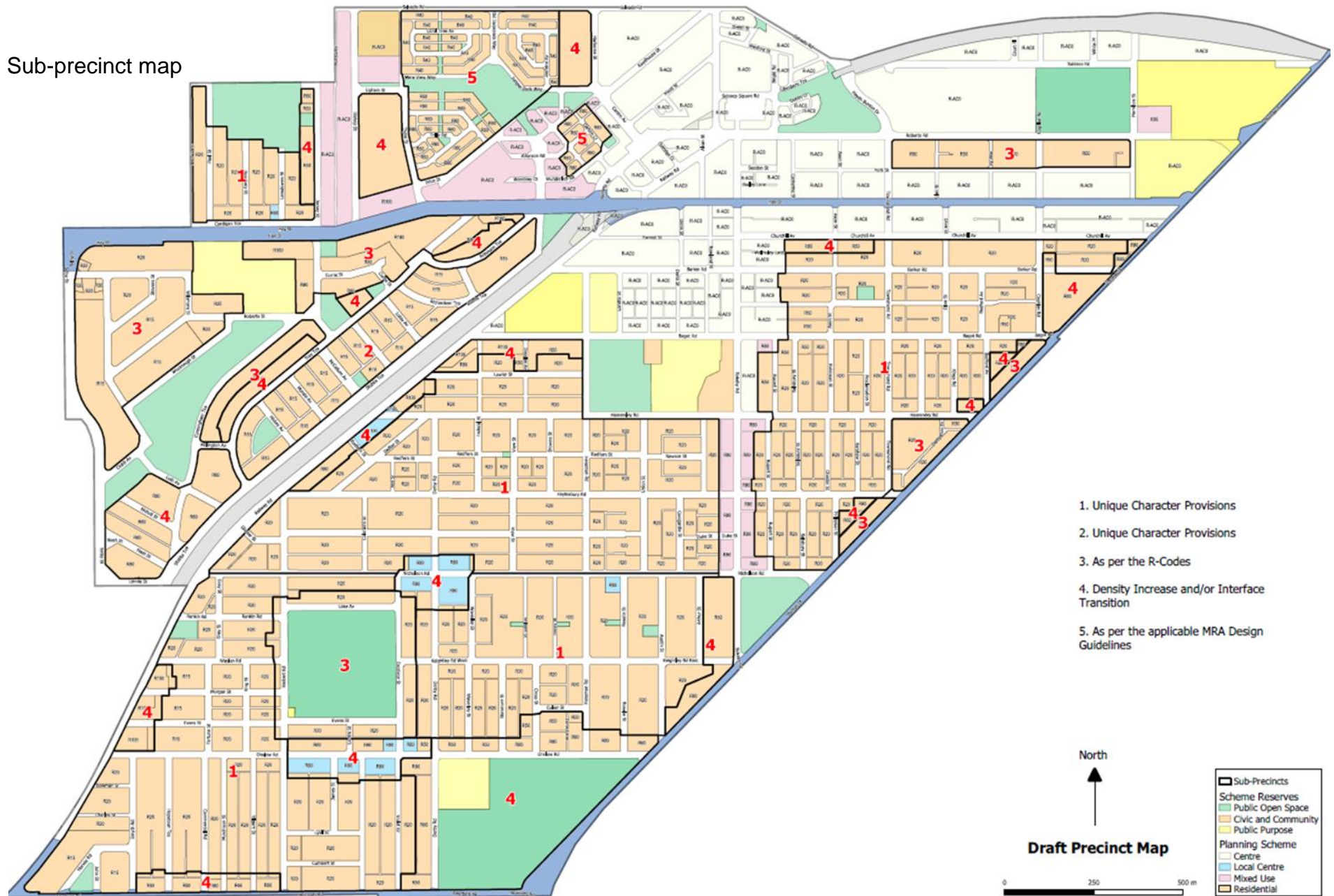
7.1 Development is to be in accordance with the provisions of the Scheme, R-Codes and site specific design guidelines.

## 8.0 Precincts

### 8.1 Precinct map

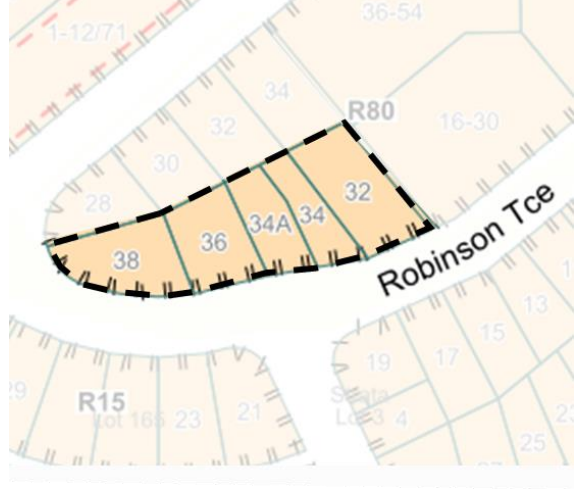
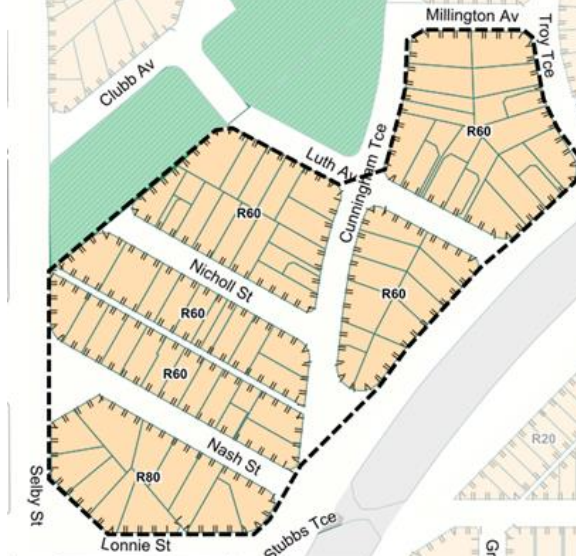
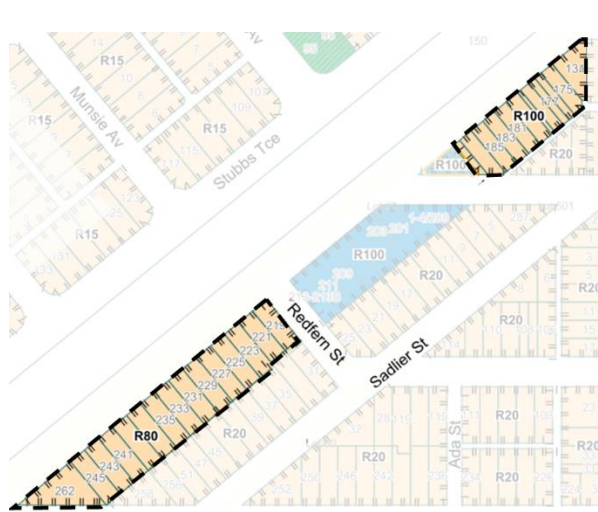
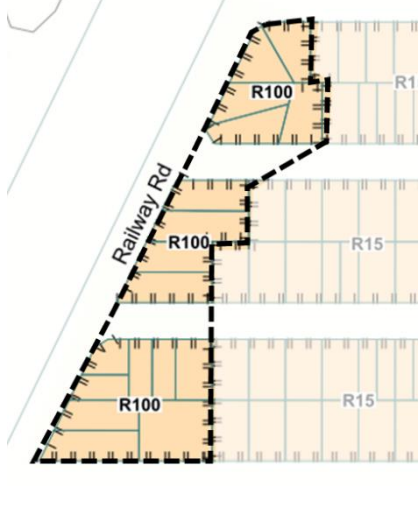
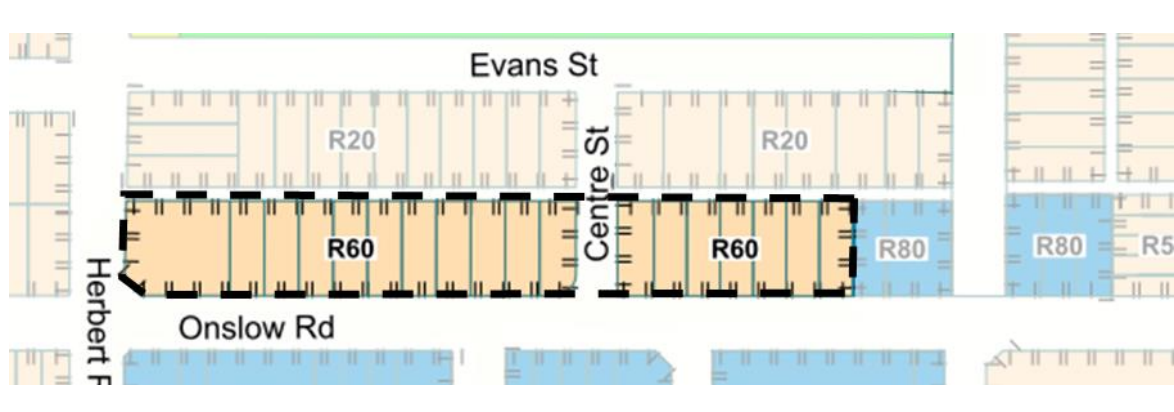


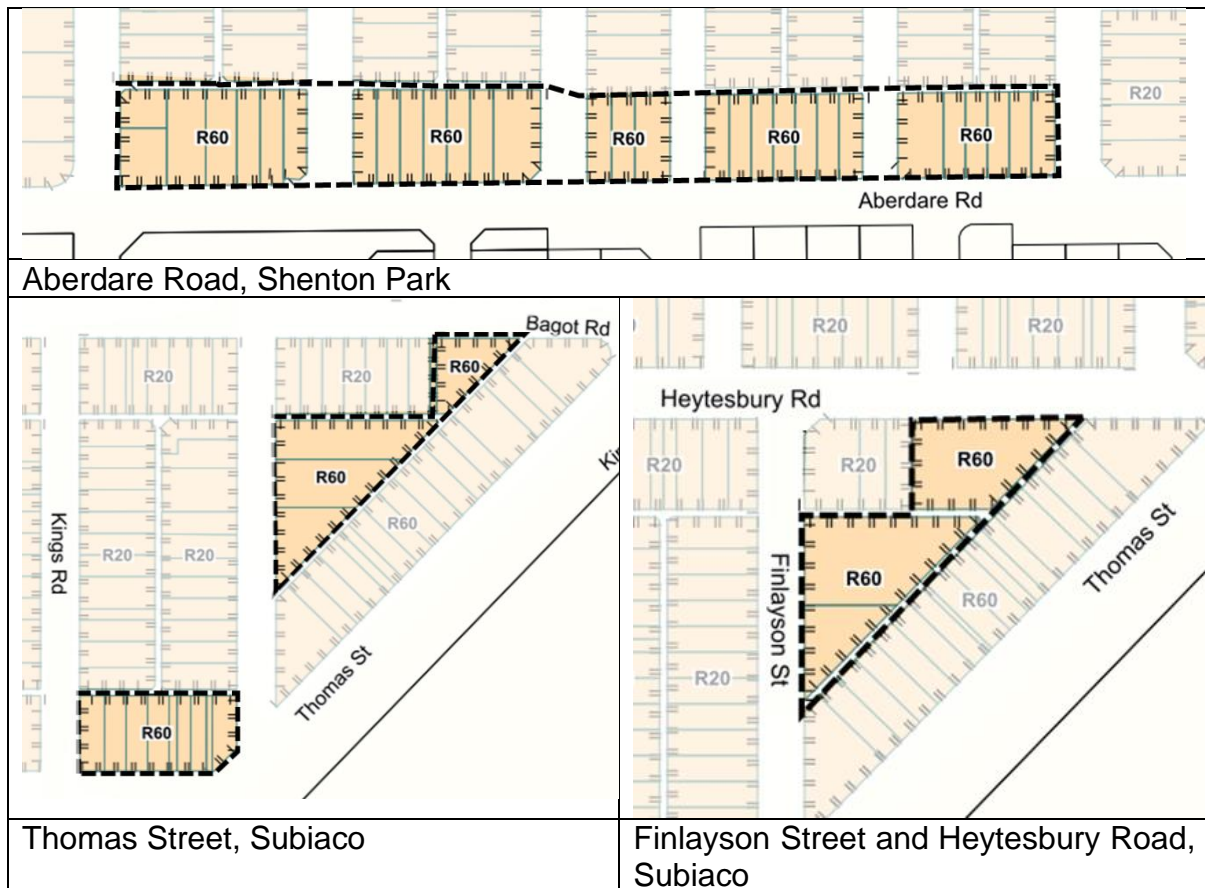
## 8.2 Sub-precinct map





### 8.3 Transition area map

|  |   |
|--|---|
|     |   |
| Robinson Terrace, Daglish  | Southern Daglish  |
|    |  |
| Railway Road, Subiaco  | Railway Road, Shenton Park  |
|  |   |
| Onslow Road, Shenton Park  |   |



## 8.4 Precinct character statements

### 8.4.1 Jolimont Precinct

The Jolimont Precinct is a predominately residential area located adjacent to Mabel Talbot Park. It was originally developed as a single parcel of land known as the Jolimont Estate in 1891. Consequently, it displays a uniform subdivision pattern, including rear laneways, and houses a collection of workers' timber cottages from the Federation period.

Apart from the medium density development along Jersey Street, the Jolimont Precinct reflects the scale and character of its original subdivision period. Some redevelopment of the original housing has occurred and although not sympathetic to the simpler timber cottages in all cases, a largely single storey, cottage style urban environment has been retained. Overall, the Precinct features intimate streetscapes with low fences, relatively small front setbacks and minimal side setbacks, with on-site car parking generally accessed via rear laneways. Some new development adjacent Mabel Talbot Park are larger two-storey dwellings, designed to take advantage of views of the park.

### 8.4.2 Daglish Precinct

The Daglish Precinct is a predominantly residential area immediately north and west of the Perth to Fremantle railway line. A number of distinct areas of development are evident.

Western Daglish is characterised by more contemporary suburban development developed in the 1960s and 1970s on University Endowment land. This area has predominantly low density dwellings which are consistent in scale and character with car-orientated design and poor pedestrian amenity.

Higher density, rental flats were developed in the Jersey Street and Troy/Cunningham Terrace areas during the 1960s. This area contains a consistent pattern of apartments at similar setbacks however the area lacks in environmental quality, surveillance and aesthetics.

Inter-war housing, which was mostly built during the 1930s, is located close to the railway line. This area has been designed on garden suburb principles with leafy streets and a highly connected grid system of street blocks. Lots here are typically larger in size and characterised by large, open front setback areas. There is access to on-site parking from existing crossovers from the primary street, and some car ports have been constructed over time. A network of rear sealed laneways also exists within this sub-precinct providing opportunities for alternative vehicular access.

A mix of more intensive commercial and residential development exists along Hay Street.

#### 8.4.3 West Subiaco Precinct

The West Subiaco Precinct is a large, predominantly residential area with the Town Centre to the north and the Perth to Fremantle railway line to the west. The Precinct has a variety of housing types, but is mostly made up of single houses on small lots. The street system is laid out in a conventional grid pattern, with narrow rear laneways being a common feature.

Many single houses constructed during the Federation and Interwar periods remain in existence today, with the relatively intact streetscapes giving the Precinct a distinctive and attractive suburban character. New replacement houses have appeared throughout the West Subiaco Precinct over time. These are of one or two storeys, and have generally been respectful of the original house forms and scale, with upper floors often setback further from the primary street.

The northern part of the Precinct on the southern side of Bagot Road has a more pronounced urban character influenced by its proximity to the nearby King Edward Memorial Hospital and the Subiaco town centre. This small area contains a variety of medium density housing types that act as a transition between the differing intensities of development.

While traditional low front fencing is characteristic of the Precinct, solid screen walls and car ports have been constructed more recently in some places to the detriment of streetscapes and the amenity of passing pedestrians.

#### 8.4.4 Triangle Precinct

The Triangle Precinct is a predominately residential area, bounded mainly by major and arterial roads. The orientation of Thomas Street, the Precinct's eastern boundary, provides its distinctive triangular shape.

The Precinct was originally developed following the gold rush of the 1880's and many of the original Federation housing has been retained and renovated to a high standard. The original pattern of development remains largely intact and includes small narrow lots, short front setbacks, low fences, rear laneways and tree lined streets.

The Triangle Precinct makes an important contribution to the diversity of the city's housing stock. Along with Federation houses on small lots, there are examples of grouped and multiple dwellings dating from throughout the last century. Also evident within the precinct are recently constructed dwellings of all types.

#### 8.4.5 Shenton Park Precinct

The Shenton Park Precinct is a large residential area bounded in most part by major roads. The Precinct has a variety of accommodation types, but is predominantly made up of single houses on mostly small lots, particularly in the area where old workers houses remain south of Onslow Road. The street system is laid out in a conventional grid pattern, with narrow rear laneways and an absence of crossovers and vehicle access to the front. The narrow lots and small lot sizes create an intimate character and quiet streets that encourage pedestrian activity.

The Precinct reflects development that mostly occurred over the period 1900 to 1939, however multi-residential development and replacement dwellings have occurred in some areas over time. These are generally respectful of the original cottage forms and scale. The lots located around the edges of Lake Jualbup are steadily being redeveloped with more substantial two storey dwellings, taking advantage of the attractive views over the lake and associated landscaped areas.

The narrow tree lined streets, Lake Jualbup, and Rosalie Park provide for green space that give the precinct its green leafy feel.

#### 8.4.6 Desirable Future Character

Areas expected to change in character due to a change in density under LPS 5 should be developed to provide a pedestrian friendly interface with the public realm and encourage walkability. Design of new development, whether apartments or townhouses, should be contemporary whilst being respectful to the existing character of nearby low-density residential areas.

Fine grained façade detailing and an opportunity for interaction, permeability and passive surveillance between the public and the private realm are expected, with front fences and landscaping being integrated into the design of the building and contributing to a sense of place. Materials and finishes of new development should be of high quality and respectful to the character typically associated with the area and incorporate organic materials such as

brick, limestone and timber without an over reliance on glass, steel and rendered finishes.

Street setback areas should be visually part of the streetscape, should be landscaped, allow for a separation of the public and private realm and should be consistent along the same side of the street.

Where an interface exists between different densities, higher density development should be respectful of its low-density neighbours by scaling development down to provide an appropriate transition. Development may be designed in such a way to displace mass further from lower density areas, while managing the potential for undue adverse impacts to the amenity of other properties.